

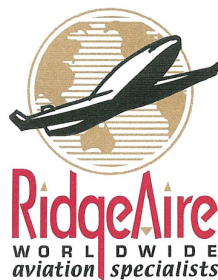
# N617CC

## 1980 Cessna 421C

---

# FAA Form 337's

**Aircraft S/N: 421C-0897**



*Prepared by the worldwide aviation specialists at RidgeAire, Inc.*



U.S. Department  
of Transportation  
Federal Aviation  
Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
2/28/2011

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation (49 U.S.C. §46301(a)).

1. Aircraft	Nationality and Registration Mark 617CC	Serial No. 421C-0897	
	Make Cessna	Model 421C	Series 400
2. Owner	Name (As shown on registration certificate) Vivace International Corp	Address (As shown on registration certificate)	
		Address 4 Dominion Dr Blvd 4 Ste 250	
		City San Antonio	State TX
		Zip 78257	Country USA

### 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME		(As described in item 1 above)	
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency	
Name Air Impressions, Inc.		U.S. Certificated Mechanic	Manufacturer
Address 7929 Karl May Dr.		Foreign Certificated Mechanic	C. Certificate No.
City Waco	State TX	<input checked="" type="checkbox"/> Certificated Repair Station	AK6R541N Airframe Class III Power Plant Class I
Zip 76708	Country USA	Certificated Maintenance Organization	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B ☒ Signature/Date of Authorized Individual Bobby Parker 03/20/2015

### Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is ☒ APPROVED ☐ REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Maintenance Organization	Person Approved by Canadian Department of Transport
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Inspection Authorization	Other (Specify)
Certificate or Designation No. AK6R541N		Signature/Date of Authorized Individual Bobby Parker 03/20/2015		

## NOTICE

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

### 8. Description of Work Accomplished

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

N617CC

Nationality and Registration Mark

03/20/2015

Date

Installed fiberglass dome hub caps I/AW FAA Approved Premiere-Aviations Master Document List No. PAS400HC-MDL Rev. "B" dated 10/29/2014 per STC SA02430LA.

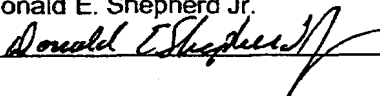
Installed external AFT body strakes I/AW FAA Approved Aircraft Performance Modifications Master Dwg. List as listed on AML No SA01935LA per STC SA01935LA and 8110-3 dated 11/7/2013.

New weight and balance computed.

Pertinent details are on file under work order #3963.

END

☒ Additional Sheets Are Attached

U.S. DEPARTMENT OF TRANSPORTATION FEDERAL AVIATION ADMINISTRATION <b>STATEMENT OF COMPLIANCE WITH AIRWORTHINESS STANDARDS</b>		1. DATE November 7, 2013
<b>AIRCRAFT OR AIRCRAFT COMPONENT IDENTIFICATION</b>		
2. MAKE Cessna	3. MODEL NO. *See Below	4. TYPE (Aircraft, Engine, Propeller, etc.) Airplane
5. NAME OF APPLICANT Aircraft Performance Modification Colorado Springs, CO.		
<b>LIST OF DATA</b>		
6. IDENTIFICATION  Engineering Change Order :  EO 1 dated 11/01/2013 for G3-050504-001 Rev K  Structures Note:	7. TITLE  * Models: 335, 340, 340A, 401, 401A, 401B, 402, 402A, 402B, 402C, 411, 411A, 414, 414A, 421, 421A, 421B, 421C, 425, 441.  G3-050504-001 Cessna 400 Series Aft Body Strakes Rev K dated 04/15/2013  1) Approval limited to Structural aspects (Statics and Fatigue) only.  2) The CR3212-4 are structurally satisfactory replacements for NAS1097AD4 rivets.  3) The Report No. HAAS-050504-001SA Rev (G) Stress Analysis remains structurally satisfactory without change.	
8. PURPOSE OF DATA To provide type data for FAA approval of structure in support of updates to fasteners in the aft strake installation. Ref FAA STC No. SA01935LA		
9. APPLICABLE REQUIREMENTS (List specific sections) Ref. TCDS 3A25, A28CE, A7CE 14CFR Part 23 through amendment 23-21, Paragraphs: 23.601 Amdt 23-0, 23.603 Amdt 23-0, 23.605 Amdt 23-0, 23.609 Amdt 23-0, 23.611 Amdt 23-7, 23.613 Amdt 23-0, 23.615(a) Amdt 23-7, 23.625 Amdt 23-7, 23.627 Amdt 23-0.		
10. CERTIFICATION - Under authority vested by direction of the Administrator and in accordance with conditions and limitations of appointment under 14 CFR Part 183, data listed above and on attached sheets numbered <u>n/a</u> have been examined in accordance with established procedures and found to comply with applicable requirements of the Airworthiness Standards listed.  <div style="display: flex; justify-content: space-between;"> <div> <input type="checkbox"/> Recommend approval of these data            I (we) Therefore         </div> <div> <input checked="" type="checkbox"/> Approve these data         </div> </div>		
11. SIGNATURE(S) OF DESIGNATED ENGINEERING REPRESENTATIVE(S) Donald E. Shepherd Jr. 	12. DESIGNATION NUMBERS(S) DERT-230307-CE	13. CLASSIFICATION(S) Structures



# ENGINEERING CHANGE ORDER

IAL - 1001  
11-74

SHEET 1 OF 1

DWG. TITLE G3-050504-001 Cessna 400 Series Aft Body Strakes Rev K dated 04/15/2013					DWG. NO. G3-050504-001		MOD. NO. EO 1	
DWN Jerry Lowe		DESIGN	CHECK	MECH.	ELECT.	STRESS D. Shepherd	GROUP LEAD	DATE Nov. 1, 2013

REASON FOR CHANGE

G3-050504-001 Rivet Substitution

DESCRIPTION OF CHANGE

MODIFICATION TO G3-050504-001 Cessna 400 Series Aft Body Strake  
Note

1) Note 7: ADD

Permissible to substitute NAS1097AD4-3 rivets with CR3212-4  
using proper countersink.

PLOT TIME:

USERNAME:

ALL OTHER INFORMATION REMAINS UNCHANGED.

*D. Shepherd*



US Department  
of Transportation  
Federal Aviation  
Administration

**MAJOR REPAIR AND ALTERATION**  
**(Airframe, Powerplant, Propeller, or Appliance)**

Form Approved  
OMB No. 2120-0020  
2/28/2011

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

1. Aircraft	Nationality and Registration Mark <b>USA N617CC</b>	Serial No. <b>421C0897</b>	
	Make <b>Cessna</b>	Model <b>421C</b>	Series
2. Owner	Name (As shown on registration certificate) <b>Aerologistics I, LLC</b>	Address (As shown on registration certificate) Address <b>3103 9th Ave Dr NW</b>	
		City <b>Hickory</b> State <b>NC</b>	Zip <b>28601</b> Country <b>USA</b>

**3. For FAA Use Only**

The technical data identified herein has been found to comply with applicable airworthiness requirements and is hereby approved for use only on the above described aircraft, subject to conformity inspection by a person authorized under §43.7

05/06/2011  
Date  
Aviation Safety Inspector  
FAA-AEA-FSDO-68

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME		(As described in Item 1 above)	
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type Manufacturer		

**6. Conformity Statement**

A. Agency's Name and Address		B. Kind of Agency	
Name <b>Riverhawk Aviation</b>		<input type="checkbox"/> U. S. Certificated Mechanic	<input type="checkbox"/> Manufacturer
Address <b>3103 9th Ave Dr NW</b>		<input type="checkbox"/> Foreign Certificated Mechanic	C. Certificate No.
City <b>Hickory</b> State <b>NC</b>		<input checked="" type="checkbox"/> Certificated Repair Station	
Zip <b>28601</b> Country <b>USA</b>		<input type="checkbox"/> Certificated Maintenance Organization	<b>FFGR870D</b>

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <b>John B Teague</b> <b>05/06/2011</b>
--	---

**7. Approval for Return to Service**

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is ☒ Approved ☐ Rejected

BY	FAA Flt. Standards Inspector	Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Inspection Authorization	

Certificate or Designation No. <b>CRF# FFGR870D</b>	Signature/Date of Authorized Individual <b>John B Teague</b> <b>05/09/2011</b>
--	---

## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

USA N617CC

Nationality and Registration Mark

05-06-11

Date

Removed the following equipment:

1 ea Garmin GTX 330 Transponder p/n 011-00455-00 with mounting rack.

Installed the following equipment:

1 ea Garmin GTX 327 Transponder p/n 011-00490-00 with mounting rack.

The GTX 327 Transponder was installed in the center instrument panel position vacated by the GTX 330 Transponder. The existing GTX 330 Transponder wiring and antenna were reused. The existing 5 amp transponder circuit breaker was removed and a new 3 amp transponder circuit breaker was installed. The GTX 327 Transponder was interfaced to the GNS 530 Com/Nav/GPS, the AR-850 blind encoder, and existing transponder antenna.

This is a follow on installation to STC Number SA00870WI issued to Garmin International for installation of a GTX 327 Transponder in a Piper PA-32 aircraft with the following deviation; 1. Aircraft make and model for this installation is a Cessna 421C.

All work was performed IAW Garmin GTX 327 Transponder Installation Manual No. 190-00877-02, Rev. N, April 2008; Cessna 421C Maintenance Manual No. D2515-23-13, Rev. 23, January 6 2003; AC 43.13-1B sections 5 through 17; and AC 43.13-2A sections 21,22,24 and 27.

An EMI test was conducted and the installed equipment does not interfere with any other installed system.

An Airplane Flight Manual Supplement was installed in the aircraft Pilots Operating Handbook.

Instructions for Continued Airworthiness and wire diagram are attached to this FAA form 337.

The aircraft equipment list was amended and the new aircraft weight & balance was computed and entered in the aircraft records.



Additional Sheets Are Attached

RIVERHAWK AVIATION INC.  
3103 9<sup>TH</sup> AVENUE DRIVE NW  
HICKORY, NC 28601  
CRS FFGR870D

FLIGHT MANUAL SUPPLEMENT

FOR

CESSNA MODEL 421C

WITH

GARMIN GTX 327 TRANSPONDER

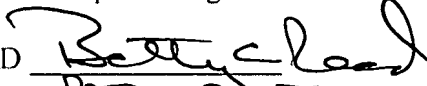
REGISTRATION NO. N617CC

SERIAL NO. 421C0897

This supplement shall be attached to the Cessna model 421C Flight Manual when the Garmin GTX 327 Transponder is installed in accordance with FAA Form 337, dated 05-06-11

The information contained herein supplements the information of the basic Flight Manual. For procedures, and performance data not contained in this supplement, consult the basic FAA approved Airplane Flight Manual.

APPROVED

  
PAT. AEA-FSDO-68

05/06/2011

## LOG OF REVISIONS

<u>Rev. No.</u>	<u>Description</u>	<u>Pages Revised</u>	<u>Approved by</u>	<u>Date</u>
-----------------	--------------------	----------------------	--------------------	-------------

## TABLE OF CONTENTS

<u>Section</u>	<u>Page</u>
I General.....	1
II Limitations.....	1
III Emergency Procedures.....	1
IV Normal Procedures.....	1
V Performance.....	1

## Section I – General

The Garmin GTX 327 panel mounted Transponder is a radio transmitter and receiver that operates on radar frequencies, receiving ground radar or TCAS interrogations at 1030 MHz. The GTX 337 is equipped with IDENT capability that activates the Special Position Identification (SPI) pulse for 18 seconds

In addition to displaying the code, reply symbol and mode of operation, the GTX 327 screen will display pressure altitude, density altitude, and timer functions, depending on equipment connections and configuration selection.

## Section II – Limitations

There is no change to the airplane limitations when this avionic equipment is installed.

## Section III – Emergency Procedures

There is no change to the airplane emergency procedures when this avionic equipment is installed.

## Section IV – Normal Procedures

Normal transponder operating procedures are described in the Garmin GTX 327 Pilot's Guide, p/n 190-00187-00, Rev. A, or later.

## Section V – Performance

There is no change to the airplane performance when this avionic equipment is installed.

## **INSTRUCTIONS FOR CONTINUED AIRWORTHINESS**

**A/C Make:** Cessna      **Model:** 421C      **S/N:** 421C0897      **Reg. #:** N617CC

**Revision:** Original      **Date:** 05-06-11

**System:** Garmin GTX 327 Transponder

### **Item**

1. This installation provides Transponder Mode A and C output capabilities when interrogated by ATC radar.
2. This installation consists of a GTX 327 Transponder mounted in the center instrument panel interfaced with the GNS 530 Com/Nav/GPS, the aircrafts blind encoder and transponder antenna.
3. Operation information can be found in the Garmin GTX 327 Airplane Flight Manual Supplement and the Garmin GTX 327 Pilots Guide p/n 190-00187-00 Rev. A or later.
4. There is no special servicing required for this installation.
5. Other than regulatory periodic functional checks the GTX 327 is an on condition component.
6. Troubleshooting information for the GTX 327 can be found in the Garmin GTX 327 Transponder Installation Manual Number 190-00187-02 latest revision.
7. Reference the Garmin GTX 327 Transponder Installation Manual Number 190-00187-02 latest revision for removal and replacement information.
8. There are no special servicing diagrams for this installation.
9. There are no special inspection requirements for this installation.
10. There are no protective treatments required for this installation.
11. Installation information can be found in the Garmin GTX 327 Transponder Installation manual number 190-00187-02 latest revision.
12. There are no special tools required for this installation.
13. This is not a commuter category aircraft.



## **INSTRUCTIONS FOR CONTINUED AIRWORTHINESS**

**A/C Make:** Cessna      **Model:** 421C      **S/N:** 421C0897      **Reg. #:** N617CC

**Revision:** Original      **Date:** 05-06-11

**System:** Garmin GTX 327 Transponder

### **Item**

14. There are no recommended overhaul periods for this installation.
15. There are no Airworthiness limitations for this installation.
16. If a revision to this ICA becomes necessary a letter will be submitted to the local FSDO with a copy of the revised FAA form 337 and revised ICA.



U.S. Department of  
Transportation  
Federal Aviation  
Administration

**MAJOR REPAIR AND ALTERATION**  
**(Airframe, Powerplant, Propeller, or Appliance)**

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act 1958)

1. Aircraft	Nationality and Registration Mark <b>N617CC</b>	Serial No. <b>421C0897</b>		
	Make <b>CESSNA</b>	Model <b>421C</b>	Series <b>421C</b>	
2. Owner	Name (As shown on registration certificate) <b>FAIRMONT AVIATION INC.</b>		Address (As shown on registration certificate)	
			Address <b>1660 PALMOUR DRIVE, SUITE AA-5</b>	
			City <b>GAINESVILLE</b>	State <b>GA</b>
			Zip <b>30501-6809</b>	Country <b>USA</b>

**3. For FAA Use Only**

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial Number
<input checked="" type="checkbox"/>	<input type="checkbox"/>	AIRFRAME	_____	(As described in Item 1 above)	_____
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

**6. Conformity Statement**

A. Agency's Name and Address		B. Kind of Agency	
Name <b>JAMES W. STOIA</b>		<input checked="" type="checkbox"/> U.S. Certificated Mechanic	<input type="checkbox"/> Manufacturer
Address <b>8604 HIGHWAY 260</b>		<input type="checkbox"/> Foreign Certificated Mechanic	C. Certificate No.
City <b>MANNING</b> State <b>SC</b>		<input type="checkbox"/> Certificated Repair Station	<b>266046100</b>
Zip <b>29102</b> Country <b>USA</b>		<input type="checkbox"/> Certificated Maintenance Organization	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <b>JAMES W. STOIA JULY 08, 2008</b>
--	--

**7. Approval for Return to Service**

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is ☒ APPROVED ☐ REJECTED

BY	FAA Flt Standards Inspector	Manufacturer	Maintenance Organization	Person Approved by Canadian Department of Transport
	FAA Designee	Repair Station	X Inspection Authorization	Other (Specify)
Certificate or Designation No. <b>266046100 IA</b>		Signature/Date of Authorized Individual <b>JAMES W. STOIA JULY 08, 2008</b>		

## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N617CC

Nationality and Registration Mark

JULY 08, 2008

Date

REPAIRED FUSELAGE - LOWER FWD BY REPLACING THE FOLLOWING NEW CESSNA PARTS: SKIN - RH LOWER FWD 5113001-20, SKIN - LH 5213001-15, STRINGER - LH 5113109-54, STRINGER - RH 5113109-55, ANGLE - LH 5213040-5, STRINGER ASSY - RH 5113109-38, STRINGER - CENTER LOWER FWD 5213001-8, DOUBLER - LH 5213045-3, BULKHEAD 5213021-3, BULKHEAD 5213026-7, BULKHEAD ASSY 5213025-21, STIFFENER 5213044-14, CHANNEL 5113109-56.

ALL WORK PERFORMED IN ACCORDANCE WITH CESSNA SERVICE MANUAL, STRUCTURAL REPAIR SECTION, CHAPTER 15, 15-30 PAGE 1-3/4 AND AC 43.13-1B/2A SECTION 4, PARAGRAPHS 4-50, 4-52 THRU 4-58.

-----END-----

☐ Additional Sheets Are Attached



US Department  
of Transportation  
Federal Aviation  
Administration

**MAJOR REPAIR AND ALTERATION**  
**(Airframe, Powerplant, Propeller, or Appliance)**

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

1. Aircraft	Nationality and Registration Mark <u>N617CC</u>	Serial No. <u>421C0897</u>		
	Make <u>CESSNA</u>	Model <u>421C</u>	Series	
2. Owner	Name (As shown on registration certificate) <u>FAIRMONT AVIATION INC</u>		Address (As shown on registration certificate)	
			Address <u>6065 ROSWELL RD NE STE 2400</u>	
			City <u>ATLANTA</u>	State <u>GA</u>
			Zip <u>30328-4011</u>	Country <u>USA</u>

**3. For FAA Use Only**

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input type="checkbox"/>	AIRFRAME		(As described in Item 1 above)	
<input type="checkbox"/>	<input checked="" type="checkbox"/>	POWERPLANT	Teledyne Continental Motors	GTSIO-520-L	292401-R
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

**6. Conformity Statement**

A. Agency's Name and Address		B. Kind of Agency	
Name <u>RAM Aircraft, Limited Partnership</u>		<input type="checkbox"/> U. S. Certificated Mechanic	<input type="checkbox"/> Manufacturer
Address <u>7505 Karl May Drive</u>		<input type="checkbox"/> Foreign Certificated Mechanic	C. Certificate No.
City <u>Waco</u> State <u>Texas</u>		<input checked="" type="checkbox"/> Certificated Repair Station	Airframe Class III, Powerplant Class I VA1R551K
Zip <u>76708</u> Country <u>United States</u>		<input type="checkbox"/> Certificated Maintenance Organization	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel  
per 14 CFR Part 43  
App. B ☐

Signature/Date of Authorized Individual

Daniel L. Bailey 3/24/08

**7. Approval for Return to Service**

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is ☒ Approved ☐ Rejected

BY	FAA Fit. Standards Inspector	Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee <input checked="" type="checkbox"/>	Repair Station	Inspection Authorization	

Certificate or  
Designation No.  
VA1R551K

Signature/Date of Authorized Individual

Daniel L. Bailey 3/24/08

### NOTICE

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

#### 8. Description of Work Accomplished

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

N617CC

Nationality and Registration Mark

3/24/08

Date

Engine modified per Dwg. 1514, Rev. R dated 6/29/05 I/AW STC SE8338SW.

Relocated Turbo Oil Supply Line I/AW RAM Dwg. No. 1224, Rev. H, dated 11/18/03 and installed locknuts on cylinder attachment studs I/AW Dwg. 1517, Rev. G dated 11/01/07 per STC SE8338SW.

Installation mechanic must complete Block 1 and 2 on reverse side and mail one copy to the Federal Aviation Administration, Aircraft Registration Branch AFS-750, P.O. BOX 25504, Oklahoma City, Oklahoma 73125.

Negligible weight and balance change.

Customer furnished with FAA approved Overhaul and Parts Manual Supplement(s) with instructions for continued airworthiness for all alterations.

Pertinent details of the above installations are on file under project no. 4031/43588.

—End—

☐ Additional Sheets Are Attached

United States Of America  
Department of Transportation - Federal Aviation Administration  
**Supplemental Type Certificate**

*Number* SE8338SW

*This Certificate issued to* RAM Aircraft, Limited Partnership  
7505 Karl May Drive  
Waco, TX 76708

*certifies that the change in the type design for the following product with the limitations and conditions therefor as specified hereon meets the airworthiness requirements of Part 13 of the CAR Regulations.*

*Original Product Type Certificate Number:* E7CE  
*Makes:* Teledyne  
*Model:* GTS10-520

*Description of Type Design Change:*

Install locknuts on cylinder attachment studs and thru-bolts, and install six additional cylinder attachment studs onto engine crankcases as detailed by RAM Drawing No. 1514 dated 08/19/91, or later FAA approved revision.

*Limitations and Conditions:*

Compatibility of this modification with previously installed equipment must be determined by installer. If the holder agrees to permit another person to use this certificate to alter the product, the holder shall give the other person written evidence of that permission.

*This certificate and the supporting data which is the basis for approval shall remain in effect until surrendered, suspended, revoked or a termination date is otherwise established by the Administrator of the Federal Aviation Administration.*

*Date of application:* October 01, 1991

*Date reissued:* October 8, 2001

*Date of issuance:* May 19, 1992

*Date amended:*



*By direction of the Administrator*

*Jean E. Priester for*  
(Signature)  
S. Frances Cox, Manager  
Special Certification Office,  
Southwest Region

(Title)



US Department  
of Transportation  
Federal Aviation  
Administration

**MAJOR REPAIR AND ALTERATION**  
(Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a))

1. Aircraft	Nationality and Registration Mark <u>N617CC</u>	Serial No. <u>421C0897</u>		
	Make <u>CESSNA</u>	Model <u>421C</u>	Series	
2. Owner	Name (As shown on registration certificate) <u>FAIRMONT AVIATION INC</u>		Address (As shown on registration certificate)	
			Address <u>6065 ROSWELL RD NE STE 2400</u>	
			City <u>ATLANTA</u>	State <u>GA</u>
			Zip <u>30328-4011</u>	Country <u>USA</u>

3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input type="checkbox"/>	AIRFRAME		(As described in Item 1 above)	
<input type="checkbox"/>	<input checked="" type="checkbox"/>	POWERPLANT	Teledyne Continental Motors	GTSIO-520-L	272484-R
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency	
Name <u>RAM Aircraft, Limited Partnership</u>		<input type="checkbox"/> U. S. Certificated Mechanic	<input type="checkbox"/> Manufacturer
Address <u>7505 Karl May Drive</u>		<input type="checkbox"/> Foreign Certificated Mechanic	C. Certificate No.
City <u>Waco</u> State <u>Texas</u>		<input checked="" type="checkbox"/> Certificated Repair Station	Airframe Class III, Powerplant Class I VA1R551K
Zip <u>76708</u> Country <u>United States</u>		<input type="checkbox"/> Certificated Maintenance Organization	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <u>Daniel L. Bailey 3/24/08</u>
--	--

7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is ☒ Approved ☐ Rejected

BY	FAA Fit. Standards Inspector	Manufacturer	Maintenance Organization	Persons Approved by Canadian Department of Transport
	FAA Designee <input checked="" type="checkbox"/>	Repair Station	Inspection Authorization	Other (Specify)

Certificate or Designation No. <u>VA1R551K</u>	Signature/Date of Authorized Individual <u>Daniel L. Bailey 3/24/08</u>
---	--

### NOTICE

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

#### 8. Description of Work Accomplished

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

N617CC

Nationality and Registration Mark

3/24/08

Date

Engine modified per Dwg. 1514, Rev. R dated 6/29/05 I/AW STC SE8338SW.

Relocated Turbo Oil Supply Line I/AW RAM Dwg. No. 1224, Rev. H, dated 11/18/03 and installed locknuts on cylinder attachment studs I/AW Dwg. 1517, Rev. G dated 11/01/07 per STC SE8338SW.

Installation mechanic must complete Block 1 and 2 on reverse side and mail one copy to the Federal Aviation Administration, Aircraft Registration Branch AFS-750, P.O. BOX 25504, Oklahoma City, Oklahoma 73125.

Negligible weight and balance change.

Customer furnished with FAA approved Overhaul and Parts Manual Supplement(s) with instructions for continued airworthiness for all alterations.

Pertinent details of the above installations are on file under project no. 4031/44911.

—End—

☐ Additional Sheets Are Attached



United States Of America  
Department of Transportation - Federal Aviation Administration

# Supplemental Type Certificate

Number SE8338SW

This Certificate issued to RAM Aircraft, Limited Partnership  
7505 Karl May Drive  
Waco, TX 76708

certifies that the change in the type design for the following product with the limitations and conditions therefor as specified hereon meets the airworthiness requirements of Part 13 of the CAR Regulations.

Original Product Type Certificate Number: E7CE  
Make: Teledyne  
Model: GTS10-520

### Description of Type Design Change

Install locknuts on cylinder attachment studs and thru-bolts, and install six additional cylinder attachment studs onto engine crankcases as detailed by RAM Drawing No. 1514 dated 08/19/91, or later FAA approved revision.

### Limitations and Conditions

Compatibility of this modification with previously installed equipment must be determined by installer. If the holder agrees to permit another person to use this certificate to alter the product, the holder shall give the other person written evidence of that permission.

This certificate and the supporting data which is the basis for approval shall remain in effect until surrendered, suspended, revoked or a termination date is otherwise established by the Administrator of the Federal Aviation Administration.

Date of application: October 01, 1991

Date reissued: October 8, 2001

Date of issuance: May 19, 1992

Date amended:



By direction of the Administrator

Jürgen E. Priester for  
(Signature)  
S. Frances Cox, Manager  
Special Certification Office,  
Southwest Region

(Title)



U.S. Department  
of Transportation  
Federal Aviation  
Administration

**MAJOR REPAIR AND ALTERATION**  
**(Airframe, Powerplant, Propeller, or Appliance)**

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title CFR 43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 44701). Failure to report can result in a civil penalty for each such violation (49 U.S.C. 46301(a)).

1. Aircraft	Nationality and Registration Mark <b>N617CC</b>	Serial No. <b>421C-0897</b>	
	Make <b>CESSNA</b>	Model <b>421C</b>	Series
2. Owner	Name (As shown on registration certificate) <b>FAIRMONT AVIATION INC</b>	Address (As shown on registration certificate) Address <b>6065 ROSWELL RD NE STE 2400</b>	
		City <b>ATLANTA</b> State <b>GA</b>	Zip <b>30328-4011</b> Country

**3. For FAA Use Only**

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME		(As described in item 1 above)	
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type Manufacturer		

**6. Conformity Statement**

A. Agency's Name and Address		B. Kind of Agency	
Name <b>AARON P BURKHART</b>		<input checked="" type="checkbox"/> U.S. Certificated Mechanic	Manufacturer
Address <b>5013 VERANDA CT</b>		Foreign Certificated Mechanic	C. Certificate No.
City <b>FLOWERY BRANCH</b> State <b>GA</b>		Certificated Repair Station	<b>AP2887269IA</b>
Zip <b>30542</b> Country <b>USA</b>		Certificated Maintenance Organization	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per  
14 CFR Part 43 App. B ☐

Signature/Date of Authorized Individual

*Aaron P. Burkhardt*

**5-22-2008**

**7. Approval for Return To Service**

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is ☒ APPROVED ☐ REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Maintenance Organization	Person Approved by Canadian Department of Transport
	FAA Designee	Repair Station	<input checked="" type="checkbox"/> Inspection Authorization	Other (Specify)

Certificate or Designation No. **AP2887269IA** Signature/Date of Authorized Individual *Aaron P. Burkhardt* **5-22-2008**

## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N617CC

Nationality and Registration Mark

5-22-2008

Date

Installed STC # SA10185SC -- Hartzell Propellers

Installed Hartzell Model HC-C3YN-2UF/FC9587DB-7 Propellers in accordance with RAM Drawing No. 2584, Rev A, dated 20 Jan 2003. FAA approved RAM Airplane Flight Manual Supplement AFMS 1097, Rev A dated 24 July 2007 inserted into Airplane Flight Manual. All work done IAW AC43.13-1B and 2A

Weight and Balance Updated this date.

\*\*\*\*\* NOTHING FOLLOWS \*\*\*\*\*

☐ Additional Sheets Are Attached

United States Of America  
Department of Transportation - Federal Aviation Administration

# Supplemental Type Certificate

*Number* SA10185SC

*This Certificate issued to* RAM Aircraft, Limited Partnership  
7505 Karl May Drive  
Waco, TX 76708

*certifies that the change in the type design for the following product with the limitations and conditions therefor as specified hereon meets the airworthiness requirements of Part 3 of the Civil Air Regulations.*

*Original Product Type Certificate Number:* A7CE

*Make:* Cessna

*Model:* 421C

*Description of Type Design Change:*

Installation of Hartzell model HC-C3YN-2UF/FC9587D-7 or HC-C3YN-2UF/FC9587DB-7 propellers in accordance with RAM Drawing No. 2584, Revision A, dated January 20, 2003, or later FAA approved revision. FAA approved RAM Airplane Flight Manual Supplement AFMS 1097, Initial Release, dated August 7, 2003, or later FAA approved revision is required.

*Limitations and Conditions:*

Compatibility of this design change with previously approved modifications must be determined by the installer. If the holder agrees to permit another person to use this certificate to alter the product, the holder shall give the other person written evidence of that permission.

*This certificate and the supporting data which is the basis for approval shall remain in effect until surrendered, suspended, revoked or a termination date is otherwise established by the Administrator of the Federal Aviation Administration.*

*Date of application:* May 07, 2002

*Date reissued:* August 27, 2003

*Date of issuance:* August 07, 2003

*Date amended:*



*By direction of the Administrator*

*Fred Stellas*  
(Signature)

FOR S. Frances Cox  
Manager, Special Certification Office  
Southwest Region

(Title)



U.S. Department  
of Transportation  
Federal Aviation  
Administration

**MAJOR REPAIR AND ALTERATION**  
**(Airframe, Powerplant, Propeller, or Appliance)**

Form Approved OMB No.2120-0020 11/30/2007	Electronic Tracking Number
For FAA Use Only	

INSTRUCTIONS: Print or type all entries. See Title CFR 43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 44701). Failure to report can result in a civil penalty for each such violation (49 U.S.C. 46301(a)).

1. Aircraft	Nationality and Registration Mark <b>N617CC</b>	Serial No. <b>421C-0897</b>	
	Make <b>CESSNA</b>	Model <b>421C</b>	Series
2. Owner	Name (As shown on registration certificate) <b>FAIRMONT AVIATION INC</b>		Address (As shown on registration certificate)
			Address <b>6065 ROSWELL RD NE STE 2400</b>
			City <b>ATLANTA</b> State <b>GA</b>
			Zip <b>30328-4011</b> Country

**3. For FAA Use Only**

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME		(As described in item 1 above)	
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type		
			Manufacturer		

**6. Conformity Statement**

A. Agency's Name and Address		B. Kind of Agency	
Name <b>AARON P BURKHART</b>		<input checked="" type="checkbox"/> U.S. Certificated Mechanic	Manufacturer
Address <b>5013 VERANDA CT</b>		Foreign Certificated Mechanic	C. Certificate No.
City <b>FLOWERY BRANCH</b> State <b>GA</b>		Certificated Repair Station	<b>AP2887269IA</b>
Zip <b>30542</b> Country <b>USA</b>		Certificated Maintenance Organization	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature <i>Aaron P. Burkhardt</i> Date of Authorized Individual <b>5-22-2008</b>
---	--

**7. Approval for Return To Service**

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is <input checked="" type="checkbox"/> APPROVED <input type="checkbox"/> REJECTED					
BY	FAA Fit. Standards Inspector	Manufacturer	Maintenance Organization	Person Approved by Canadian Department of Transport	
	FAA Designee	Repair Station	<input checked="" type="checkbox"/> Inspection Authorization	Other (Specify)	
Certificate or Designation No. <b>AP2887269IA</b>		Signature <i>Aaron P. Burkhardt</i> Date of Authorized Individual <b>5-22-2008</b>			

## NOTICE

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

### 8. Description of Work Accomplished

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

N617CC

Nationality and Registration Mark

5-22-2008

Date

Installed STC # SA4592SW -- Improved Engine Cooling Baffle and Improved Exhaust System Slip Joints

Installed improved engine cooling baffle and improved exhaust system slip joints according to RAM Drawings 1035 dated 1/1984 and 1153 Rev. A dated 1.23.1985. All work done in accordance with AC43.13-1B and 2A.

\*\*\*\*\* NOTHING FOLLOWS \*\*\*\*\*

☐ Additional Sheets Are Attached

United States Of America  
Department of Transportation - Federal Aviation Administration  
**Supplemental Type Certificate**

*Number* SA4592SW

*This Certificate issued to* RAM Aircraft, Limited Partnership  
7505 Karl May Drive  
Waco, TX 76708

*certifies that the change in the type design for the following product with the limitations and conditions therefor as specified hereon meets the airworthiness requirements of Part 3 of the Civil Air Regulations.*

*Original Product Type Certificate Number:* A7CE

*Make:* Cessna

*Model:* 421C

*Description of Type Design Change:*

Installation of improved engine cooling baffle and improved exhaust system slip joints according to RAM Drawings 1035 dated 1/84, and 1153 Rev. A dated 1/23/85, or later FAA approved revision.

*Limitations and Conditions:*

Compatibility of this modification with previously installed equipment must be determined by installer. If the holder agrees to permit another person to use this certificate to alter the product, the holder shall give the other person written evidence of that permission.

*This certificate and the supporting data which is the basis for approval shall remain in effect until surrendered, suspended, revoked or a termination date is otherwise established by the Administrator of the Federal Aviation Administration.*

*Date of application:* January 05, 1982

*Date reissued:* October 08, 2001

*Date of issuance:* January 27, 1982

*Date amended:* 7/24/85, 10/27/88 Rev. 2



*By direction of the Administrator*

*S. Frances Cox*  
(Signature)

S. Frances Cox, Manager  
Special Certification Office,  
Southwest Region

(Title)



U.S. Department  
of Transportation  
Federal Aviation  
Administration

**MAJOR REPAIR AND ALTERATION**  
**(Airframe, Powerplant, Propeller, or Appliance)**

Form Approved OMB No. 2120-0020 11/30/2007	Electronic Tracking Number
For FAA Use Only	

INSTRUCTIONS: Print or type all entries. See Title CFR 43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 44701). Failure to report can result in a civil penalty for each such violation (49 U.S.C. 46301(a)).

1. Aircraft	Nationality and Registration Mark <b>N617CC</b>	Serial No. <b>421C-0897</b>	
	Make <b>CESSNA</b>	Model <b>421C</b>	Series
2. Owner	Name (As shown on registration certificate) <b>FAIRMONT AVIATION INC</b>	Address (As shown on registration certificate) Address <b>6065 ROSWELL RD NE STE 2400</b> City <b>ATLANTA</b> State <b>GA</b> Zip <b>30328-4011</b> Country	

**3. For FAA Use Only**

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input type="checkbox"/>	<input checked="" type="checkbox"/>	AIRFRAME		(As described in item 1 above)	
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type Manufacturer		

**6. Conformity Statement**

A. Agency's Name and Address		B. Kind of Agency	
Name <b>AARON P BURKHART</b>	Address <b>5013 VERANDA CT</b> City <b>FLOWERY BRANCH</b> State <b>GA</b> Zip <b>30542</b> Country <b>USA</b>	<input checked="" type="checkbox"/> U.S. Certificated Mechanic	Manufacturer
		Foreign Certificated Mechanic	C. Certificate No.
		Certificated Repair Station	<b>AP2887269IA</b>
		Certificated Maintenance Organization	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B <input type="checkbox"/>	Signature/Date of Authorized Individual <i>Aaron P. Burkhardt</i>	<b>5-22-2008</b>
---	--	------------------

**7. Approval for Return To Service**

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is <input checked="" type="checkbox"/> APPROVED <input type="checkbox"/> REJECTED					
BY	FAA Fit. Standards Inspector	Manufacturer	Maintenance Organization	Person Approved by Canadian Department of Transport	
	FAA Designee	Repair Station	<input checked="" type="checkbox"/> Inspection Authorization	Other (Specify)	
Certificate or Designation No. <b>AP2887269IA</b>		Signature/Date of Authorized Individual <i>Aaron P. Burkhardt</i> <b>5-22-2008</b>			



## NOTICE

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

### 8. Description of Work Accomplished

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

N617CC

Nationality and Registration Mark

5-22-2008

Date

Installed STC # SA3721SW -- Vacuum Pump Cooling Shroud

Installed vacuum pump cooling shroud in accordance with RAM Drawing No. 1221, "Vacuum Pump Cooling Shroud Installation", Rev B, Dated 11 Feb 1987 and RAM Drawing No. 1199, "Vacuum Pump Cooling Shroud Detail", Rev. B dated 17 Feb 1987.

\*\*\*\*\* NOTHING FOLLOWS \*\*\*\*\*

☐ Additional Sheets Are Attached

United States Of America  
Department of Transportation - Federal Aviation Administration  
**Supplemental Type Certificate**

*Number* SA3721SW

*This Certificate issued to* RAM Aircraft, Limited Partnership  
7505 Karl May Drive  
Waco, TX 76708

*certifies that the change in the type design for the following product with the limitations and conditions therefor as specified hereon meets the airworthiness requirements of Part 3 of the Civil Air Regulations.*

*Original Product Type Certificate Number:* A7CE

*Make:* Cessna

*Model:* 401/402/414/421 Series

*Description of Type Design Change:*

Install vacuum pump cooling shroud in accordance with RAM Drawing No. 1221, "Vacuum Pump Cooling Shroud Installation", Revision B, dated February 11, 1987 and RAM Drawing No. 1199, "Vacuum Pump Cooling Shroud Detail", Revision B, dated February 17, 1987, or later FAA approved revisions.

*Limitations and Conditions:*

Compatibility of this design change with previously approved modifications must be determined by the installer. If the holder agrees to permit another person to use this certificate to alter the product, the holder shall give the other person written evidence of that permission.

*This certificate and the supporting data which is the basis for approval shall remain in effect until surrendered, suspended, revoked or a termination date is otherwise established by the Administrator of the Federal Aviation Administration.*

*Date of application:* July 31, 1986

*Date reissued:* October 08, 2001

*Date of issuance:* February 23, 1987

*Date amended:*



*By direction of the Administrator*

*S. Frances Cox*  
(Signature)  
S. Frances Cox, Manager  
Special Certification Office,  
Southwest Region

(Title)



U.S. Department  
of Transportation  
Federal Aviation  
Administration

# MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title CFR 43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 44701). Failure to report can result in a civil penalty for each such violation (49 U.S.C. 46301(a)).

1. Aircraft	Nationality and Registration Mark N617CC	Serial No. 421C-0897	
	Make CESSNA	Model 421C	Series
2. Owner	Name (As shown on registration certificate) FAIRMONT AVIATION INC	Address (As shown on registration certificate) Address 6065 ROSWELL RD NE STE 2400 City ATLANTA State GA Zip 30328-4011 Country	

## 3. For FAA Use Only

4. Type		5. Unit Identification			
Repair	Alteration	Unit	Make	Model	Serial No.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	AIRFRAME		(As described in item 1 above)	
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type Manufacturer		

## 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency	
Name	AARON P BURKHART	<input checked="" type="checkbox"/> U.S. Certificated Mechanic	Manufacturer
Address	5013 VERANDA CT	Foreign Certificated Mechanic	C. Certificate No.
City	FLOWERY BRANCH State GA	Certificated Repair Station	AP2887269IA
Zip	30542 Country USA	Certificated Maintenance Organization	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per 14 CFR Part 43 App. B ☐ Signature *Aaron P. Burkhardt* Date of Authorized Individual 5-22-2008

## 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is ☒ APPROVED ☐ REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Maintenance Organization	Person Approved by Canadian Department of Transport
	FAA Designee	Repair Station	<input checked="" type="checkbox"/> Inspection Authorization	Other (Specify)

Certificate or Designation No. AP2887269IA Signature *Aaron P. Burkhardt* Date of Authorized Individual 5-22-2008

## NOTICE

*Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.*

### 8. Description of Work Accomplished

*(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)*

N617CC

Nationality and Registration Mark

5-22-2008

Date

Chafe damage was found in the LH Inboard Engine Beam -- Outer Web at FS 115.00.

Repaired chafe damage per Cessna Aircraft Company Repair definition S-421C-0897/01RD, Cessna 421C Maintenance Manual, Supplemental Inspection Documents SID 54-10-04 and SID 53-10-04, and AC43.13 1B.

Blended chaffed area to a depth not greater than .018 to a RMS 63 micro-inch finish.

Eddy current inspection performed by Certified Inspection Services FAA Repair Station X18R997N, no cracks noted.

Fabricated repair doubler from .063 2024-T3 Clad Aluminum per Figure 3.2.

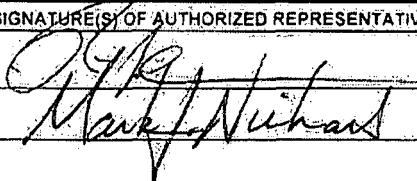
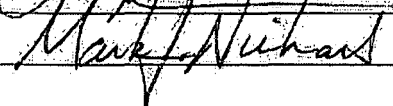
Installed repair doubler using MS20470AD5 and NAS1097AD5 rivets and MIL-S-8802 Class B2 sealant.

Reinstalled Lower nacelle skin removed to gain access to patched area using standard practices of AC43.13-1B.

See attached Cessna Aircraft Repair Definition Dated 25 April 2008.

\*\*\*\*\* NOTHING FOLLOWS \*\*\*\*\*

☒ Additional Sheets Are Attached

U.S. DEPARTMENT OF TRANSPORTATION FEDERAL AVIATION ADMINISTRATION <b>STATEMENT OF COMPLIANCE WITH AIRWORTHINESS STANDARDS</b>			FAA Project No. N/A	
<b>AIRCRAFT OR AIRCRAFT COMPONENT IDENTIFICATION</b>				
MAKE	MODEL NO.	TYPE (Aircraft, Engine, Propeller, etc.)	NAME OF APPLICANT/AUTHORIZATION NO.	
Cessna	421C	Airplane	Cessna Aircraft Company Wichita, Kansas DOA-230594-CE	
<b>LIST OF DATA</b>				
IDENTIFICATION	TITLE			
<p><b>NOTES:</b></p> <ol style="list-style-type: none"> <li>1. This Data approval is in support of Organizational Designation Project No. None and DOES NOT constitute DER approval of the data listed herein and is not valid for any other purpose or application.</li> <li>2. In accordance with the Delegation Option Authority for Cessna Aircraft Company (DOA-230594-CE), when authorized below by the stamp of approval of the Executive Engineer, the listed data is FAA approved in support of a Major Repair</li> <li>3. Approval is for design only, not installation.</li> <li>4. Reference Repair Definition File Folder 421C-0897/01 in Structures Group Files.</li> <li>5. Compatibility of this data with the aircraft configuration must be determined by the installer.</li> </ol>				
<u>Repair Definition</u> S-421C-0897/01RD N/C	REPAIR OF CHAFE DAMAGE IN THE LH INBOARD ENGINE BEAM - OUTER WEB			
<b>PURPOSE OF DATA</b> To approve listed data for repair of chafe damage to the LH inboard engine bear outer web (P/N 5054020-35) on Aircraft 421C-0897, registration number N617CC.				
<b>APPLICABLE REQUIREMENTS (List specific sections)</b>				
R. J. Lamberger			M. J. Nienhaus	
CAR	3.171	3.174	3.295	3.307
	3.172	3.292	3.296	
	3.173	3.294	3.301	Dated May 15, 1956, as amended by 3-1 through 3-5 and 3-8
<b>CERTIFICATION</b> - As directed by the Administrator and in accordance with the conditions and limitations of authorization under 14 CFR, data listed above and on attached sheets numbered _____ have been examined in accordance with established procedures and found to comply with applicable requirements of the Airworthiness Standards listed.				
<input type="checkbox"/> Recommend approval of these data				
(We) Therefore: <input checked="" type="checkbox"/> Approve these data				
SIGNATURE(S) OF AUTHORIZED REPRESENTATIVE(S)		NAME	CLASSIFICATION	DATE
 		R. J. Lamberger	Structures	4/30/08
		M. J. Nienhaus	Structures	4/30/08



CESSNA AIRCRAFT COMPANY  
P. O. BOX 7704  
WICHITA, KANSAS 67277-7704

STRUCTURES

MODEL NO: 421C REPORT NO: S-421C-0897/01RD

REPAIR DEFINITION

REPAIR OF CHAFE DAMAGE IN THE LH INBOARD ENGINE BEAM - OUTER WEB

REPORT DATE: April 25, 2008

PREPARED BY: D. C. Abel  
D. C. Abel

CHECKED BY: M. J. Nienhaus  
M. J. Nienhaus

APPROVED BY: R. J. Lamberger  
R. J. Lamberger

Cessna Aircraft Company  
P. O. Box 7704  
Wichita, KS 67277

Page i  
Report S-421C-0897/01RD  
Aircraft 421C-0897 (Unit 0897)

REVISIONS

LETTER	DATE	DESCRIPTION	BY	APPROVED
N/C	25April2008	Original release, ECR 099005.	DCA	See cover page.

## 1.0 AIRCRAFT DATA

Serial Number: 421C-0897      Unit Number: 0897

A/W Date: 08/25/1980      6013.6 Hrs / N/A Ldgs      Reg. No.: N617CC

Date of Occurrence: On or about 04/11/2008

Owner/Operator: Fairmont Aviation

Repair Facility: abAviation LLC

## SPECIAL NOTES

- This Repair Definition covers only the damage defined in this document and it is the responsibility of the repair facility to assure that the defined damage agrees with the actual damage.
- All nondestructive inspections (NDI) specified in the Repair Definition must be performed by a qualified facility and meet the requirements defined in Appendix A and Supplemental Inspection Documents SID 54-10-04 and SID 53-10-04.

The repair described in the report satisfies applicable strength and fatigue, fail safe, and/or damage tolerance requirements. This repair also satisfies applicable lightning strike requirements. Structural substantiation and back-up data for this repair are contained in Engineering's Repair Definition File Folder 421C-0897/01.

The structural repairs designed herein for this aircraft do not affect the original structural inspection criteria (procedures or timing) as published in the Cessna Model 421C Service Manual.

The repair definition contained within this document has been found to comply with the following title CAR 3 regulations:

CAR 3.171, 3.172, 3.173, 3.174, 3.292, 3.294, 3.295, 3.296, 3.301, 3.307 as amended by Amendments 3-1 through 3-5 and 3-8 dated May 15, 1956.

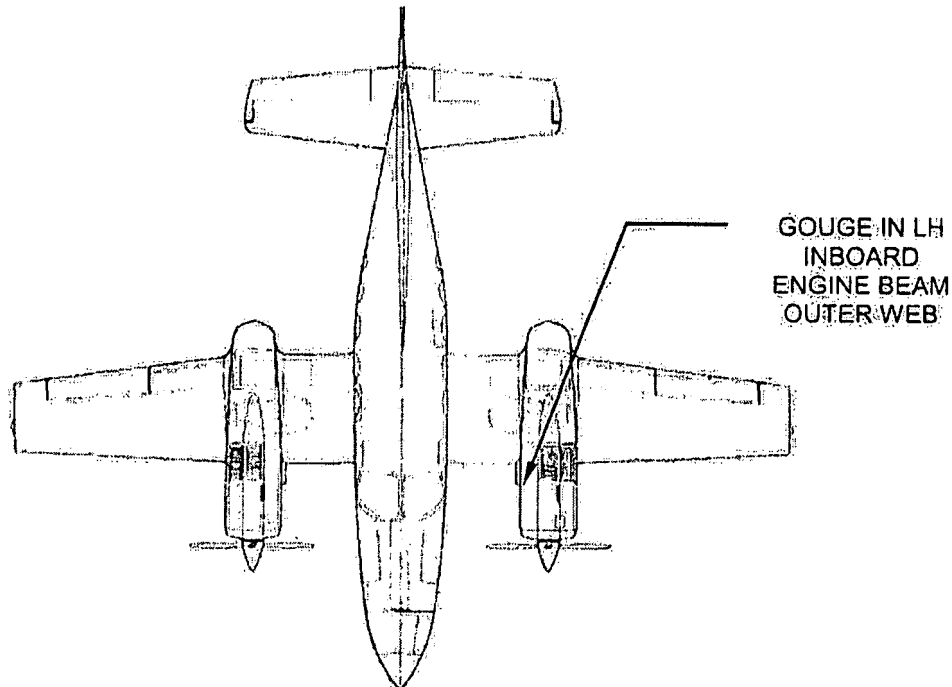


## 2.0 DAMAGE DESCRIPTION

Chafe damage was found in the LH Inboard Engine Beam – Outboard Web (P/N 5054020-35) at FS 115.00. The final blended dimensions of the chafe damage in the LH Inboard Engine Beam – Outboard Web (P/N 5054020-35) covered an area of approximately 1.375 inch long, 1.25 inch wide, and 0.018 inch in depth (at the deepest point).

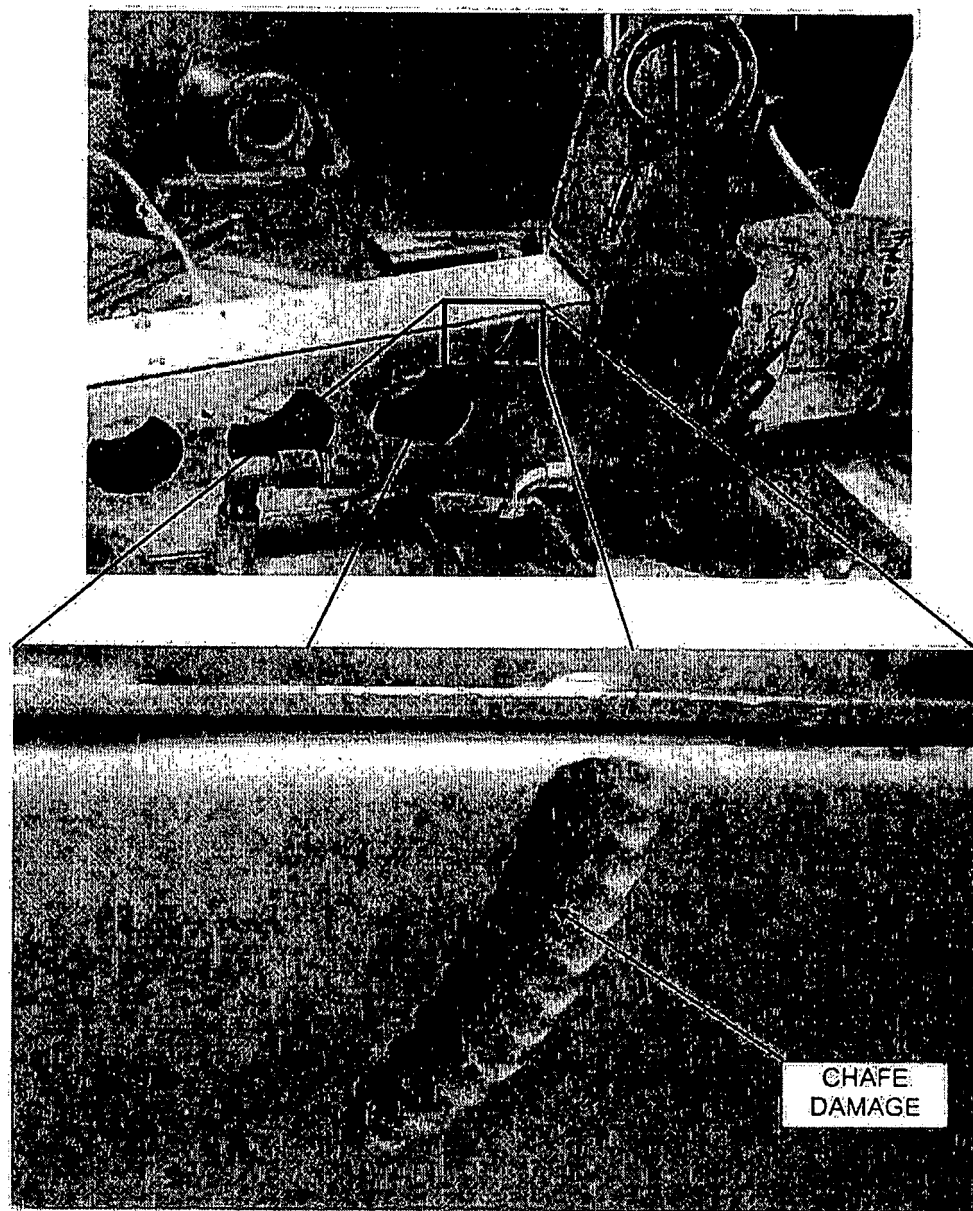
Figure 2.1 is a sketch showing the general location of the damage. Figure 2.2 are actual photographs showing the damage to the LH engine beam.

FIGURE 2.1:  
GENERAL AIRCRAFT DAMAGE LOCATION

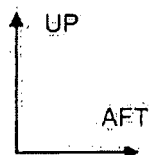


MODEL 421C AIRCRAFT

FIGURE 2.2:  
DAMAGE PHOTO - LH INBOARD ENGINE BEAM



(VIEW LOOKING INBOARD/DOWN AT INNER LH ENGINE BEAM @ FS 115.00)



### 3.0 REPAIR DEFINITION

The chafe damage in the LH Inboard Engine Beam – Outboard Web (P/N 5054020-35) of the LH Inboard Beam Assembly (P/N 5054020-31) at FS 115.00 is to be repaired by inspecting for cracks and removing the chafe damage with a blend operation and a final polish and installing a repair doubler. This repair is to be performed in conjunction with Model 421C Supplemental Inspection Documents SID 54-10-04 and SID 53-10-04 instructions. Read all directions prior to beginning the repair and contact Cessna Customer Service if the repair cannot be installed as described below.

**This repair requires that the final blended dimensions be sent back to Cessna Propeller Customer Service for further disposition.**

**Some of the following repair steps have already been performed on the original preliminary repair.**

Repair per the following instructions:

#### 3.1 REPAIR PREP

- First the LH engine will be removed from the aircraft. All weight will need to be taken off of the nacelle.
- Derivet the Lower LH Nacelle Skin Assembly (P/N 5154046-11) and the Lower Inboard LH Cap (P/N 5054020-29). This is to provide access to the inside of the LH Inboard Engine Beam Assembly (P/N 5054020-31). Remove rivets as shown in Figure 3.3 to allow installation of the -1 Repair Doubler. Note that the rivets on top of the engine beam will have to be removed also. Remove fasteners per Model 421C Service Manual.

#### 3.2 INSPECT FOR DAMAGE

- Surface eddy current inspect for cracks in an approximate 1.0 inch region around the damaged location in the LH Inboard Engine Beam – Outboard Web (P/N 5054020-35) as shown in Figure 3.1.
  - Surface eddy current inspect per the attached eddy current document (Reference Appendix A) and Supplemental Inspection Documents SID 54-10-04 and SID 53-10-04.

- Contact Cessna Propeller Customer Service if cracks or other anomalies are detected.
- If no cracks or other anomalies are detected, continue with repair as described below.

### 3.3 REMOVAL OF DAMAGE

- Locally blend the chafe damage in the LH Inboard Engine Beam – Outboard Web (P/N 5054020-35) at FS 115.00 using 180 grit sandpaper to a minimum RMS 63 micro-inch finish as shown in Figure 3.1.
  - Blend using a 0.25 - 1.0 in. radius (perpendicular to part surface).
  - Do not blend any deeper than required to remove all surface damage.
  - Maximum blend depth shall not exceed .018 in. depth (.045) material remaining).
  - Do not blend into the edge of the LH Upper Cap (P/N 5054020-7) as shown in Figure 3.1.
  - Report remaining material thickness and depth of material removed, and length and width of blended region to Cessna Propeller Customer Service for further disposition.
  - Blending operation shall present a smooth and gradual transition between the reworked and non-reworked regions.

### 3.4 FINAL BLEND

- Locally polish the reworked area in the LH Inboard Engine Beam – Outboard Web (P/N 5054020-35) at FS 115.00 using 320 grit sandpaper to a minimum RMS 63 micro-inch finish as shown in Figure 3.1.
  - Polish using a 0.25 - 1.0 in. radius (perpendicular to part surface).
  - Do not blend any deeper than required to achieve the above surface finish.
  - Maximum blend depth shall not exceed .018 in. depth (.045) material remaining).

- Do not blend into the edge of the LH Upper Cap (P/N 5054020-7) as shown in Figure 3.1.
- Report remaining material thickness and depth of material removed, and length and width of blended region to Cessna Propeller Customer Service for further disposition.
- Blending operation shall present a smooth and gradual transition between the reworked and non-reworked regions.
- Final polish marks are to be in the Forward - Aft direction.
- Clean, chem. film and epoxy prime per Model 421C Service Documents.

### 3.5 FABRICATION OF PARTS

- Procure an S3806-12 from Cessna Aircraft Company.
- Fabricate a -1 Repair Doubler from 0.063 in. thick 2024-T3 Clad aluminum sheet (S3806-12), per AMS-QQ-A-250/5, as shown in Figure(s) 3.2 thru 3.4.
  - Actual geometry of -1 Repair Doubler may vary to incorporate the size, type, and number of fasteners shown.
  - The -1 Repair Doubler has a 0.19" Bend Radius.
  - Maintain 2D minimum edge distance to all included fasteners.
  - Break all sharp edges 0.03 to 0.06 in. radius.
  - Clean, chem. film and epoxy prime the Filler per Model 421C Service Manual, "Protective Treatment Of Metal".

**NOTE:** If defects are detected then fabricate the -1 Repair Doubler from 2024-O condition aluminum and heat treat to T3 condition per AMS-QQ-A-250/5 or procure a new 5054020-35 LH Outboard Web.

### 3.6 REPAIR INSTALLATION

- Install the -1 Repair Doubler as shown in Figure(s) 3.2 thru 3.4.
  - Install size, number, and type of fasteners shown in Figure(s) 3.2 thru 3.4.

- Maintain 2D minimum edge distance at all fastener locations and 4D-8D spacing at all added fastener locations.
- Install fasteners wet with Type I, Class B sealant per Model 421C Service Manual. Figure 3.4 illustrates the cross section of the parts stack up.
- Fay surface seal all mating surfaces per Model 421C Service Manual.
- Re-install any remaining fasteners that were removed from the Lower Skin to facilitate the installation of the Repair Doubler using B/P type and size fasteners. Install fasteners wet with Type I, Class B sealant per Model 421C Service Manual.

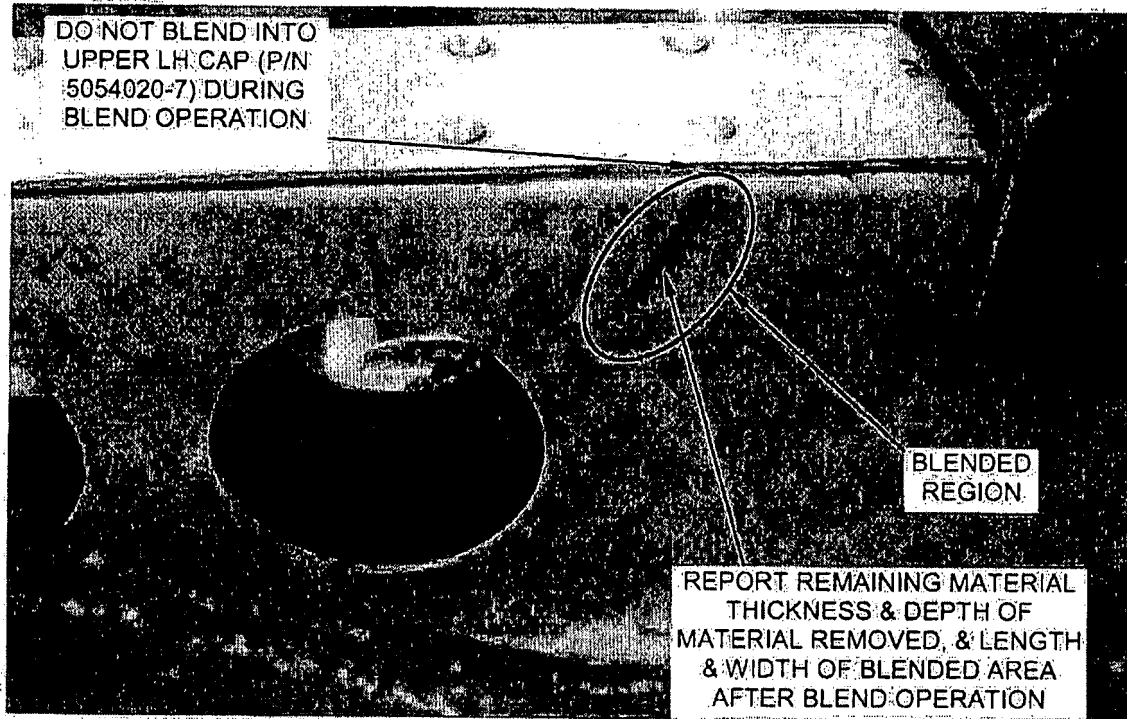
NOTE: It is permissible to substitute HL11VAZ Hi-Loks in place of NAS1097AD rivets. It is permissible to substitute HL10VAZ Hi-Loks in place of MS20470AD rivets. Use HL70 collars with above pins.

### 3.7 GENERAL

- Pre-assembly operations such as fitting, filing, drilling, dimpling and deburring shall be completed prior to cleaning.
- Fasteners are not to have an edge distance (measured from the center of hole to edge of part) of less than two times the hole diameter (2D) to the nearest part edge. The distance between fasteners is not to be less than four times the hole diameter (4D) and not more than eight times the hole diameter (8D). This note applies in all cases except where specifically noted in this report.
- Unless indicated otherwise below, all repairs and modification to the airplane are to conform to the Cessna Model 421C Service Manual.
- This repair is authorized for the stated Serial Numbered airplane only.
- This repair is for an unmodified airplane. Any non-Cessna modification that affects the aircraft gross weight, original design, or performance may invalidate published Continued Airworthiness requirements (i.e., inspection techniques and/or interval). Contact the STC Holder or originator of the modification for revised inspection criteria.

- If this Repair Definition Report (RD) is provided in support of a non-USA registered aircraft, acceptance of these findings is at the discretion of the civil aviation authority of the State of Registry. Compatibility of this data with the aircraft configuration must be determined by the installer.
- Prior to fastener removal, note the size, type, and location of any fasteners to be removed. This will facilitate later installation of similar fasteners (or oversized, as required, and as permitted by the Model 421C Service Manual or SRM).
- Seal all parts on installation, using the sealants specified in the Maintenance Manual, for each location. If no sealant or procedure is specified, Seal as instructed in the Maintenance Manual, using MIL-S-8802 Class B2 sealant on Assembly. Drive all fasteners wet with sealant.
- Break all edges to 0.03 to 0.06 inch radius, clean, chem. film and epoxy prime all bare aluminum surfaces and abrasive clean and epoxy prime all stainless steel surfaces per the applicable Model 421C Service Documentation.

FIGURE 3.1  
REPAIR DETAIL - LH ENGINE BEAM BLEND



(VIEW LOOKING INBOARD/DOWN AT INNER LH ENGINE BEAM)

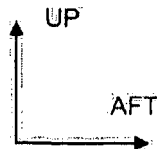




FIGURE 3.2  
REPAIR SKETCH

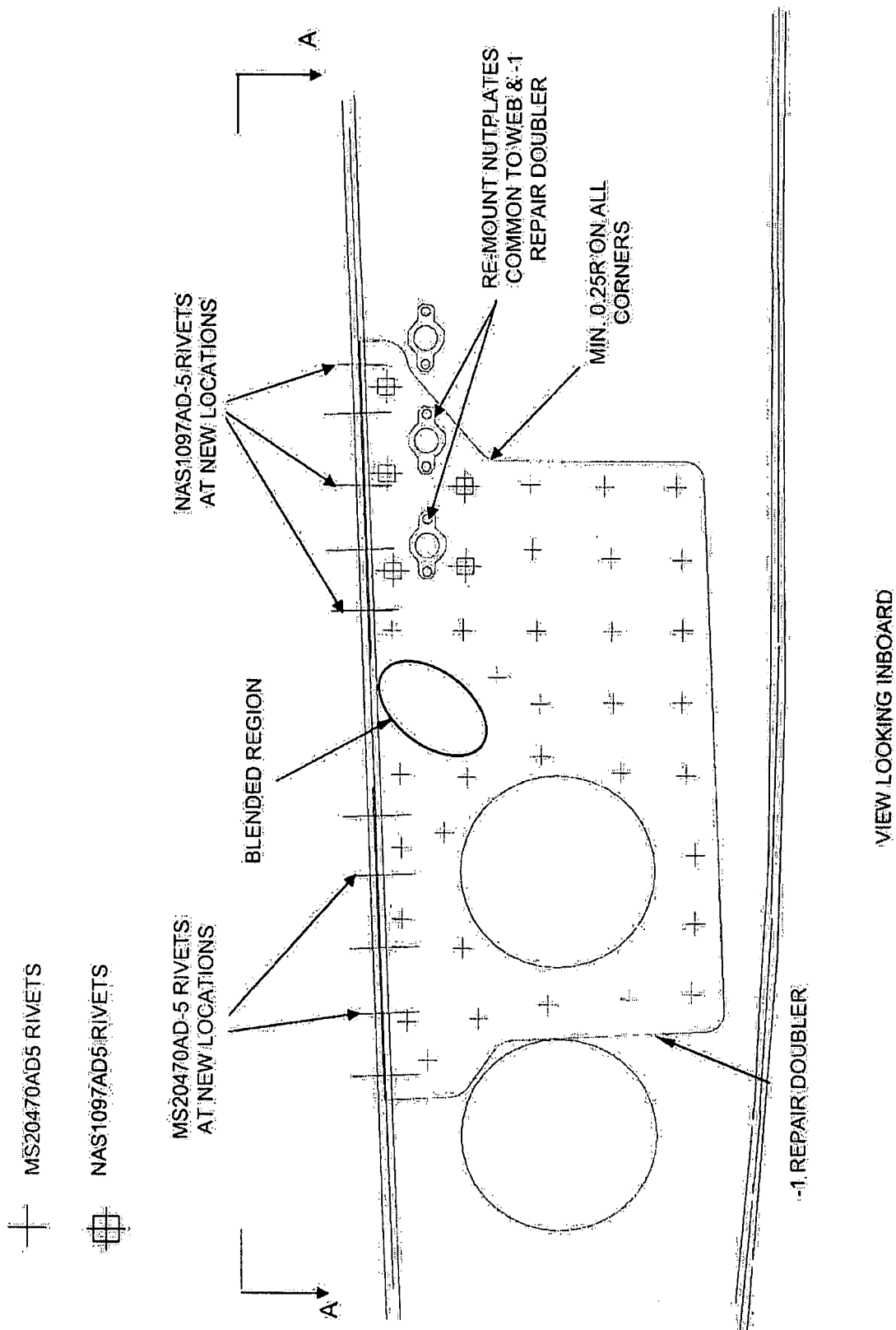


FIGURE 3.3  
REPAIR SKETCH

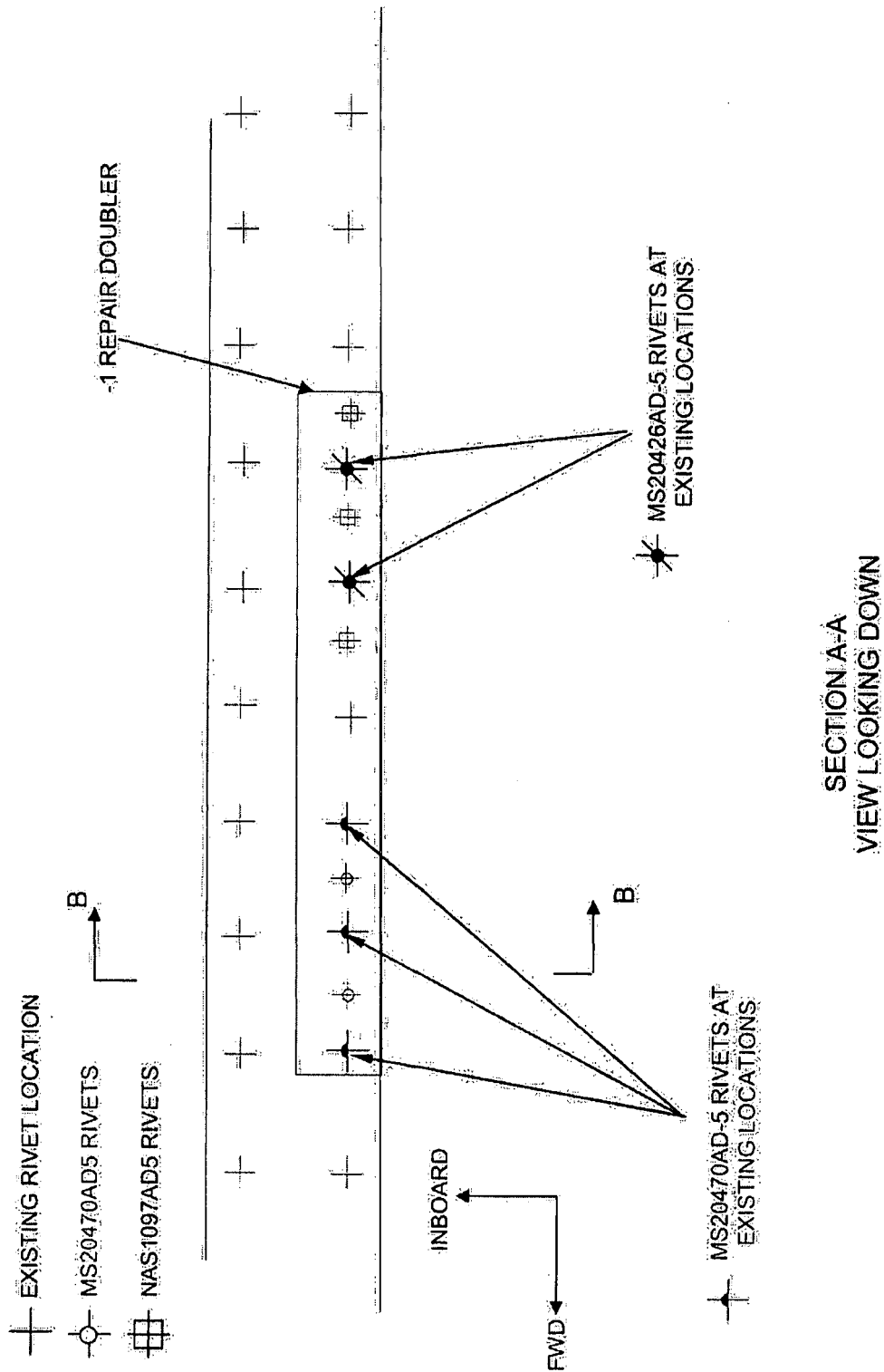
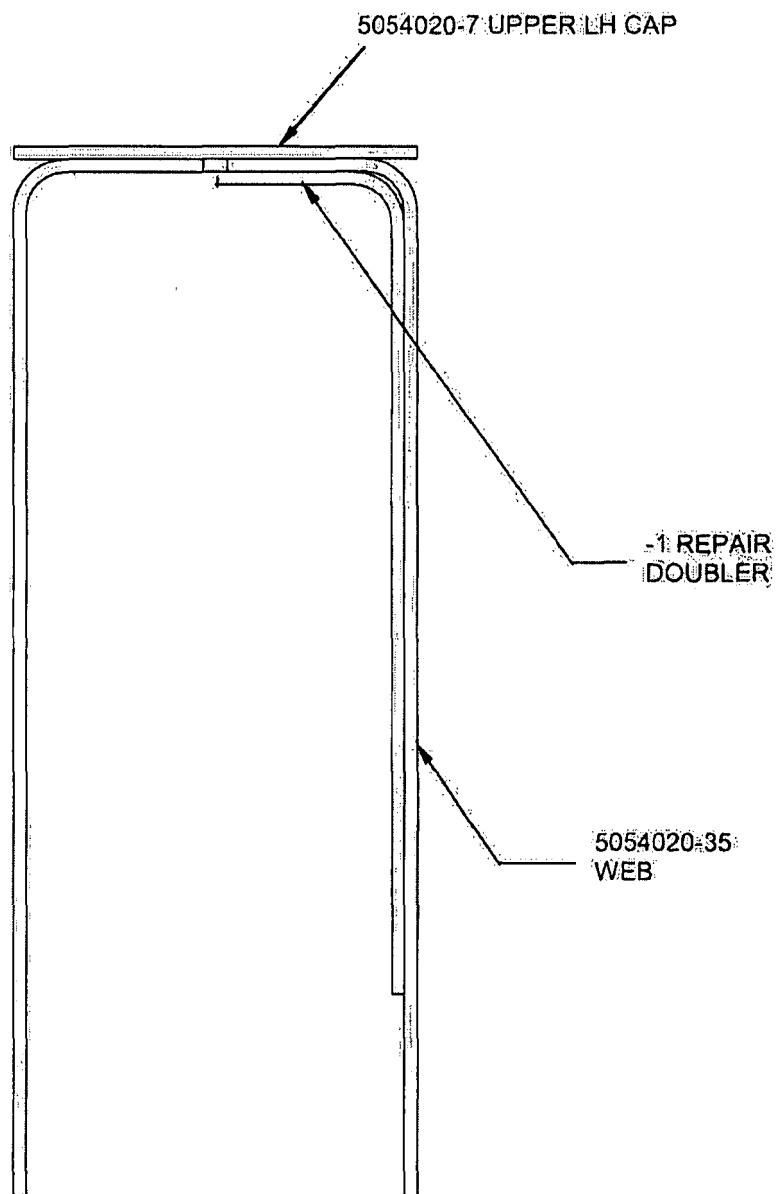


FIGURE 3.4  
REPAIR SKETCH



SECTION B-B  
VIEW LOOKING AFT

## APPENDIX A

## EDDY CURRENT INSPECTION

### **Description**

- A. Eddy current inspection is effective for the detection of surface or near surface cracks in most metals. This method is accomplished by inducing eddy currents into the part and observing electrical variations of the induced field. The character of the observed field change is interpreted to determine the nature of the defect. Eddy current inspection can be applied to airframe parts or assemblies where the inspection area is accessible to contact by the eddy current probe. An important use of eddy current inspection is the detection of cracking caused by corrosion and stress around fastener holes with the fasteners installed.

### **2. Organizational Requirements**

- A. The inspection must be performed by an FAA approved Repair Station that holds a specialized rating for Eddy Current testing
- B. Personnel Requirements.
  - I. Personnel employed by a certified facility and engaged in nondestructive inspection on Cessna propeller equipped model airplanes shall be qualified and certified in the applicable method(s).
  - II. Personnel must be certified Level II or equivalent in the applicable method(s) in accordance with the employers qualification/ certification program.

### **3. Instrument Requirements: Surface Inspection**

- A. Instrument Sensitivity:
  - 1) Certain inspection techniques require the use of instruments that provide both phase and amplitude information on a storage cathode ray tube for impedance plane analysis. Impedance plane instruments may be substituted for metered instruments. Metered instruments shall not be substituted for impedance plane instruments where the ability to distinguish phase information is required.

- 2) The instrument shall demonstrate a repeatable signal response that has a signal to noise ratio of greater than 3 to 1 for the test in which it is to be employed. Impedance plane instruments shall be able to resolve the signal within the guidelines shown in Figures 1 and 2.
- 3) The functional performance of the eddy current instrumentation shall be verified on an annual basis.

**B. Probe Sensitivity:**

- 1) The probe may have an absolute or differential coil arrangement.
- 2) The probe may be shielded or unshielded. A shielded probe is normally recommended.
- 3) The probe shall have an operating frequency that produces the required test sensitivity and depth of penetration. For a surface inspection conducted on an aluminum part, the frequency should be 200 kHz (plus or minus 50 kHz).
- 4) Smaller coil diameters are more efficient in detecting cracks. A coil diameter of 1/8 inch is normally used.
- 5) For crack detection, the coil will usually contain a ferrite core.
- 6) The probe shall not give interfering responses from handling pressures, scanning or normal operating pressure variations on the sensing coil which cause the signal to noise ratio to be less than 3 to 1.
- 7) Teflon tape may be used to decrease the wear on the eddy current probe coil. When Teflon tape is used, the instrument calibration must be verified.

**4. Instrument Calibration Standards**

- A) Nonferrous reference standards should be of an alloy having the same major base metal, basic temper and the approximate electrical conductivity of the material to be inspected.
- B) Reference standards shall have a minimum surface finish of 150 RHR or RMS 165.

- C) The dimensional accuracy of EDM notches shall be documented and traceable to the National Institute of Standards and Technology (NIST).
- D) In some cases a specially fabricated reference standard will be necessary to simulate a part's geometry, configuration, and/or the specific discontinuity location. Artificial discontinuities may be used in the reference standard.

Figure 1

Absolute Probe Calibration Range

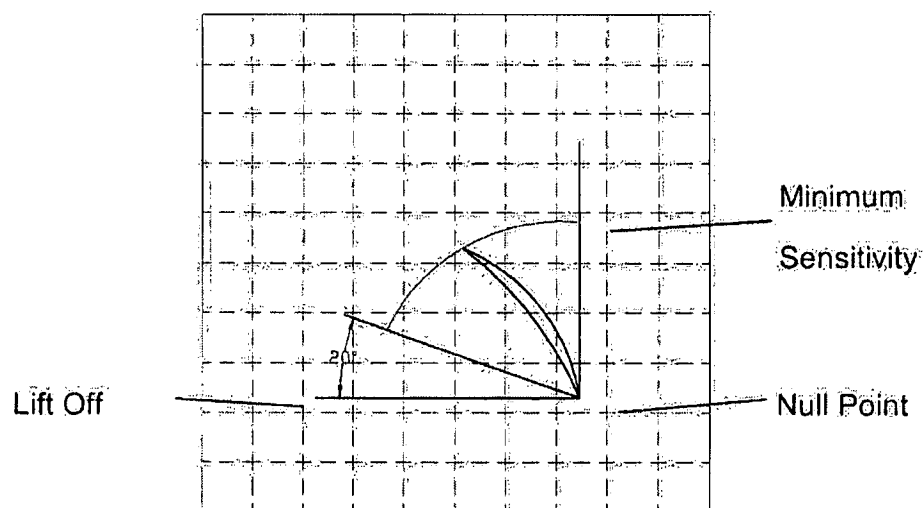
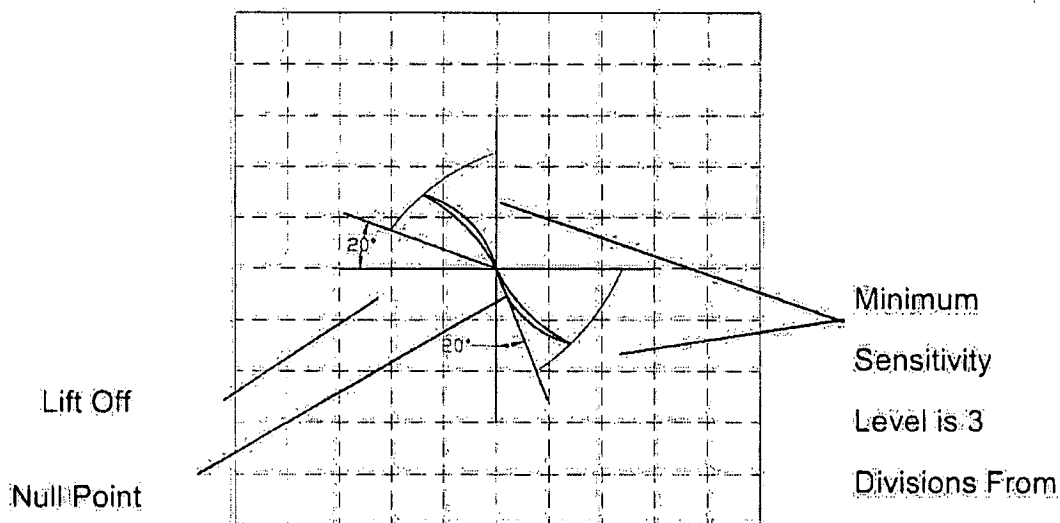


Figure 2

Differential Probe Calibration Range



### 3.8 EDDY CURRENT INSPECTION: SURFACE TECHNIQUES

#### 1. **Description**

- A. This section presents the general requirements for the use of the eddy current method for detection of surface defects. This should be used in conjunction with specific instructions regarding the location for inspection and the types of discontinuities to be detected.
- B. Ensure compliance with the requirements described in the Eddy Current Inspection: General section.

#### 2. **Inspection Considerations**

- A. **Temperature:** Inspections shall not be performed until the temperature of the probe, the standard and the material have been allowed to equalize.
- B. **Surface Condition**
  - 1) **Finish:** The surface finish of the area to be tested shall be 150 RHR or RMS 165 or finer.
  - 2) **Cleanliness:** The areas to be tested shall be free of dirt, grease, oil, or other contaminants.
  - 3) **Probe Contact:** Eddy current inspection requires that good contact be made between the probe and the part unless a specific procedure requires a setoff. Mildly corroded parts must be cleaned lightly with emery cloth, heavily corroded or painted parts must be lightly abraded and cleaned locally in the area on which the probe will be placed.

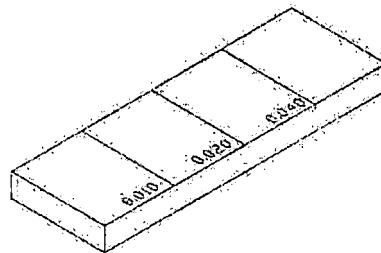


### 3. Instrument Calibration

- A. The instrument shall be calibrated and operated in accordance with this technique and the manufacturer's instructions.
- B. Instrument calibration shall be performed prior to inspection. Calibration shall be checked at intervals necessary to maintain calibration during continuous use and at the conclusion of the inspection. The instrument shall be recalibrated if any part of the system is replaced or if any calibrated control settings are changed.
- C. A 0.020-inch surface notch shall be used for surface inspections unless otherwise specified. A typical eddy current surface reference standard with EDM notch depths of 0.010 inch, 0.020 inch, and 0.040 inch is shown in Figure 3.
- D. Adjust the instrument to achieve a minimum separation of three major screen divisions between the null/balance point and the appropriate reference notch. The signal from a differential probe should be considered peak to peak.
- E. Filters may be used to improve the signal to noise ratio.

Figure 3

Typical Surface Reference Standard



#### 4. Inspection

- A. Whenever possible, the area of inspection shall be scanned in two different directions which are at scan paths 90 degrees to each other.
- B. Scan the inspection area at index increments that do not exceed the width of the eddy current test coil. The part edge shall be scanned as long as the response from edge effect does not mask the calibration notch response. Areas where edge effect is greater than the calibration notch signal shall not be inspected using eddy current.
- C. Whenever possible, fillets and radii should be scanned both transverse and parallel to the axis of the radius. The edge of the fillet or radius shall be scanned transverse to the axis of the radius.

#### 5. Interpretation

- A. If an indication is detected, carefully repeat the inspection in the opposite direction of probe movement to verify the indication. If the indication persists, carefully monitor the amount of probe movement or rotation required to cause the instrument to move off maximum indication response.
- B. When the eddy current probe is centered over a crack, the crack response will be at maximum and any movement of the probe ( $\pm 0.020$  inch,  $\pm 2$  degrees) will cause the instrument to start to return to the original reading. Corrosion pits, foreign material, and out-of-round holes can cause an instrument response for 20 to 30 degrees of bolt hole probe rotation before the indication begins to return to the original reading.
- C. Unless otherwise specified, cracks shall be considered unacceptable.
- D. The end of a crack is determined using the 50 percent method. Scan the probe slowly across the end of the crack until a point is reached where the crack signal amplitude has been reduced by 50 percent. The center of the probe coil is considered to be the end of the crack.



3755 Industrial CT., NW Suite 16 Suwanee, GA 30024 FAA Repair Station X18R997N

**REPORT OF EDDY CURRENT EVALUATION**

PROJECT: Engine Support Beams  
CLIENT: ab Aviation LLC  
SPECIFICATION: Cessna A/C Company Model 421C SSI 54-10-04  
FREQUENCY: 200 V/H: .6/1.0  
PHASE: 180 PERSIST: N/A  
GAIN: 42 SWEEP: N/A  
PROBE: Gen/Surface FILTER: N/A

EVALUATION DATE: 04/14/08  
PROJECT NUMBER: 15512  
P. O.: 1695  
ET INSTRUMENT: MIZ 21A  
CALIBRATION DATE: 6/06/08  
CALIBRATION STANDARD: Zetec .008", .020", .040"

REPORTED  
TO: Aaron Burkhart

A/C Make Cessna Model 421C S/N 421C-0897 Tail N617CC

**SKETCH AND TECHNIQUE DESCRIPTION**

Identification	ACCEPT	REJECT	Comments
# 1 Engine Beam Support Structure	√	N/A	No cracks noted @ time of inspection
# 2 Engine Beam Support Structure	√	N/A	No cracks noted @ time of inspection

Reviewed By: \_\_\_\_\_

Inspector: Kenneth Vaughn Level II



3755 Industrial CT NW Suite 16 • Suwanee, Georgia 30024 • (678) 730-2000 • (678) 482-9677 • www.mmelab.com

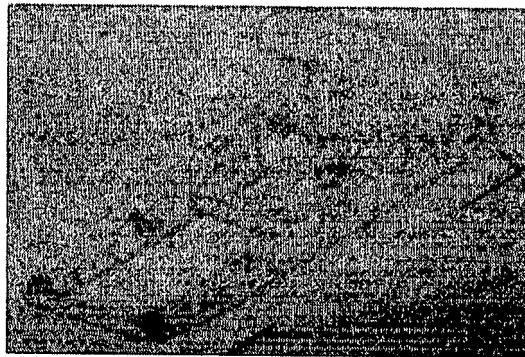
## FIELD TEST REPORT

PROJECT: UTT Left Nacelle Inbd Beam  
CLIENT: ab Aviation  
EVALUATED BY: Kenneth Vaughn  
CONTRACTOR: N/A

PROJECT NO.: 15527  
DATE OF EVALUATION: 4-18-08  
TYPE OF EVALUATION: Ultrasonic Thickness

### SUMMARY OF WORK PERFORMED:

Perform a ultrasonic thickness inspection on the left nacelle –inboard beam in the area of chaffing, readings were taken in .025" increments fwd and aft of the blended area and, at the top and bottom of area. Thickness reading taken from the backside of the center area the average thickness is approx. .050".



FDW	Center	Aft	Top	Bottom
.052	.047	.050	.053	.053
.050	.045	.052		

Client: \_\_\_\_\_ Evaluator: Kenneth Vaughn



U.S. Department  
of Transportation  
Federal Aviation  
Administration

## MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved  
OMB No. 2120-0020  
11/30/2007

Electronic Tracking Number

For FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title CFR 43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 44701). Failure to report can result in a civil penalty for each such violation (49 U.S.C. 46301(a)).

1. Aircraft	Nationality and Registration Mark N617CC	Serial No. 421C0897	
	Make Cessna	Model 421C	Series
2. Owner	Name (As shown on registration certificate) FAIRMONT AVIATION INC.	Address (As shown on registration certificate)	
		Address 6065 ROSWELL RD NE STE 2400	
		City ATLANTA	State GA
		Zip 30328-4011	Country USA

### 3. For FAA Use Only

#### 4. Type

#### 5. Unit Identification

Repair	Alteration	Unit	Make	Model	Serial No.
<input checked="" type="checkbox"/>	<input type="checkbox"/>	AIRFRAME		(As described in item 1 above)	
<input type="checkbox"/>	<input type="checkbox"/>	POWERPLANT			
<input type="checkbox"/>	<input type="checkbox"/>	PROPELLER			
<input type="checkbox"/>	<input type="checkbox"/>	APPLIANCE	Type Manufacturer		

### 6. Conformity Statement

A. Agency's Name and Address		B. Kind of Agency	
Name	JOHN H. Z. REINER	<input checked="" type="checkbox"/> U.S. Certificated Mechanic	Manufacturer
Address	706 PATRICK MILL RD. S.W.	Foreign Certificated Mechanic	C. Certificate No.
City	WINDER State GA	Certificated Repair Station	149426349
Zip	30680-3855 Country USA	Certificated Maintenance Organization	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Extended range fuel per  
14 CFR Part 43 App. B ☐

Signature/Date of Authorized Individual

4/26/2007

### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is ☒ APPROVED ☐ REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Maintenance Organization	Person Approved by Canadian Department of Transport
	FAA Designee	Repair Station	<input checked="" type="checkbox"/> Inspection Authorization	Other (Specify)
Certificate or Designation No. 149426349		Signature/Date of Authorized Individual		

4/26/2007

## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N617CC

4/26/2007

Nationality and Registration Mark

Date

REPAIRED CRACK IN SKIN @ ZONE 311 AND 312 (REF. 1). ON BOTTOM OF FUSELAGE AFT OF PRESSURE VESSEL AT LOCATION OF FLIGHT PHONE ANTENNA MOUNT (REF. 2).  
AS PER CESSNA 421 SERVICE MANUAL (1976-1985) REV. 23,  
CHAP. 15 STRUTURAL REPAIR,  
SECTION 40,  
PAGE 16,  
FIGURE 13,  
SHEET 1 (REF. 3)  
DIMENTION OF PATCH 1ea. 8 5/8" X 5 5/8" X .025 2024 T3 alclad AND 1ea. 4 5/8" X 3 3/4" X .025 2024 T3 alclad  
DIMENTION OF DOUBLER 1ea. 12" X 13 1/2" X .025 2024 T3 alclad  
RIVETED PATCH AND DOUBLER AS PER CHAPTER 15,  
SECTION 00,  
PAGES 1-14

#### ADDITIONAL SHEETS ATTACHED

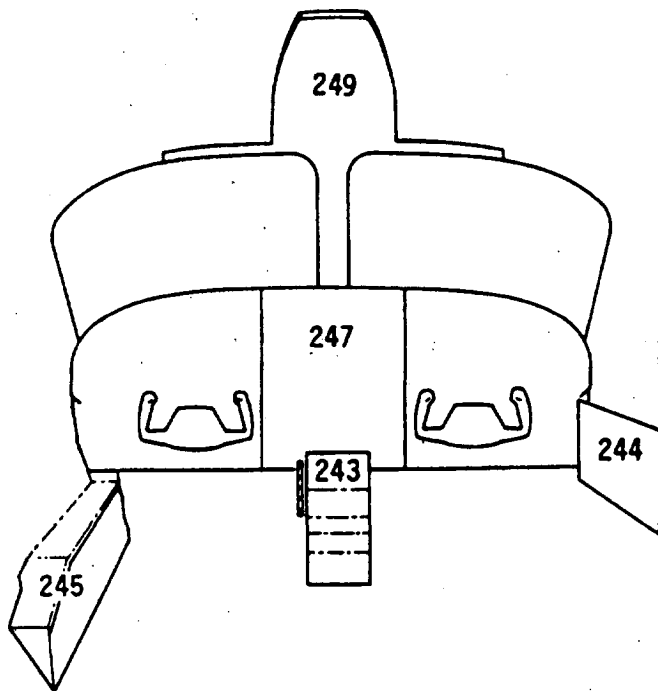
REF. 1 = 1-60 PG. 4 FIG. 1 (SHEET 2)

REF. 2 = 15-40 PG. 38 FIG. 33.

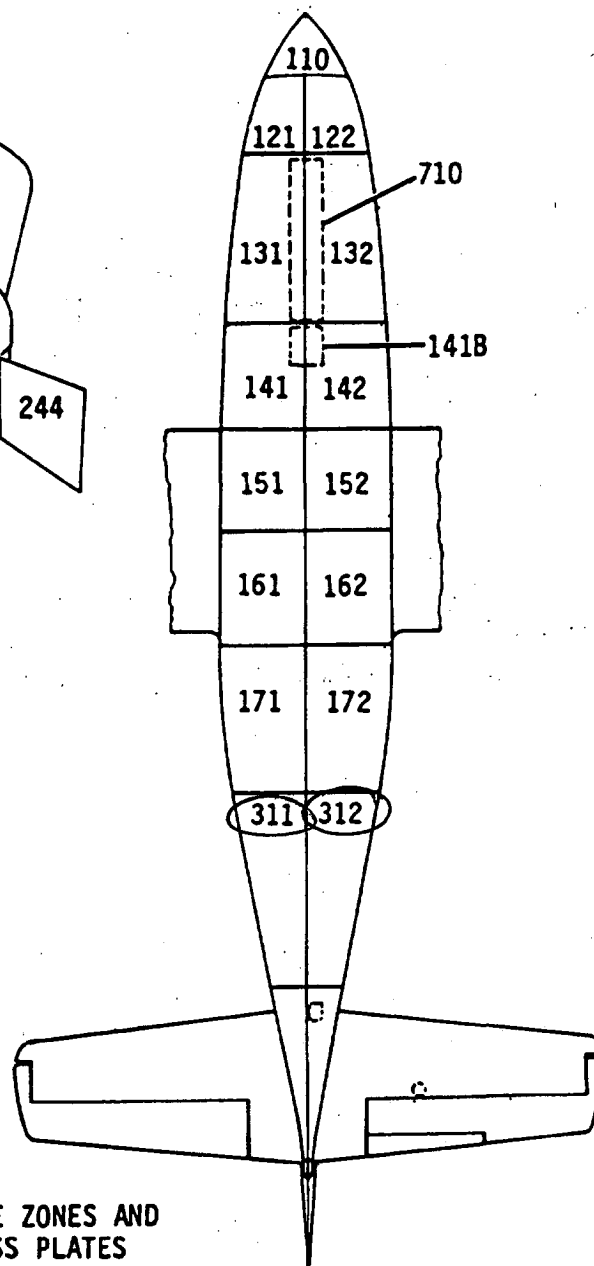
REF. 3 = 15-40 PG. 16 FIG. 13

\*\*\*\*\* NOTHING FOLLOWS \*\*\*\*\*

☒ Additional Sheets Are Attached



CREW COMPARTMENT ZONES

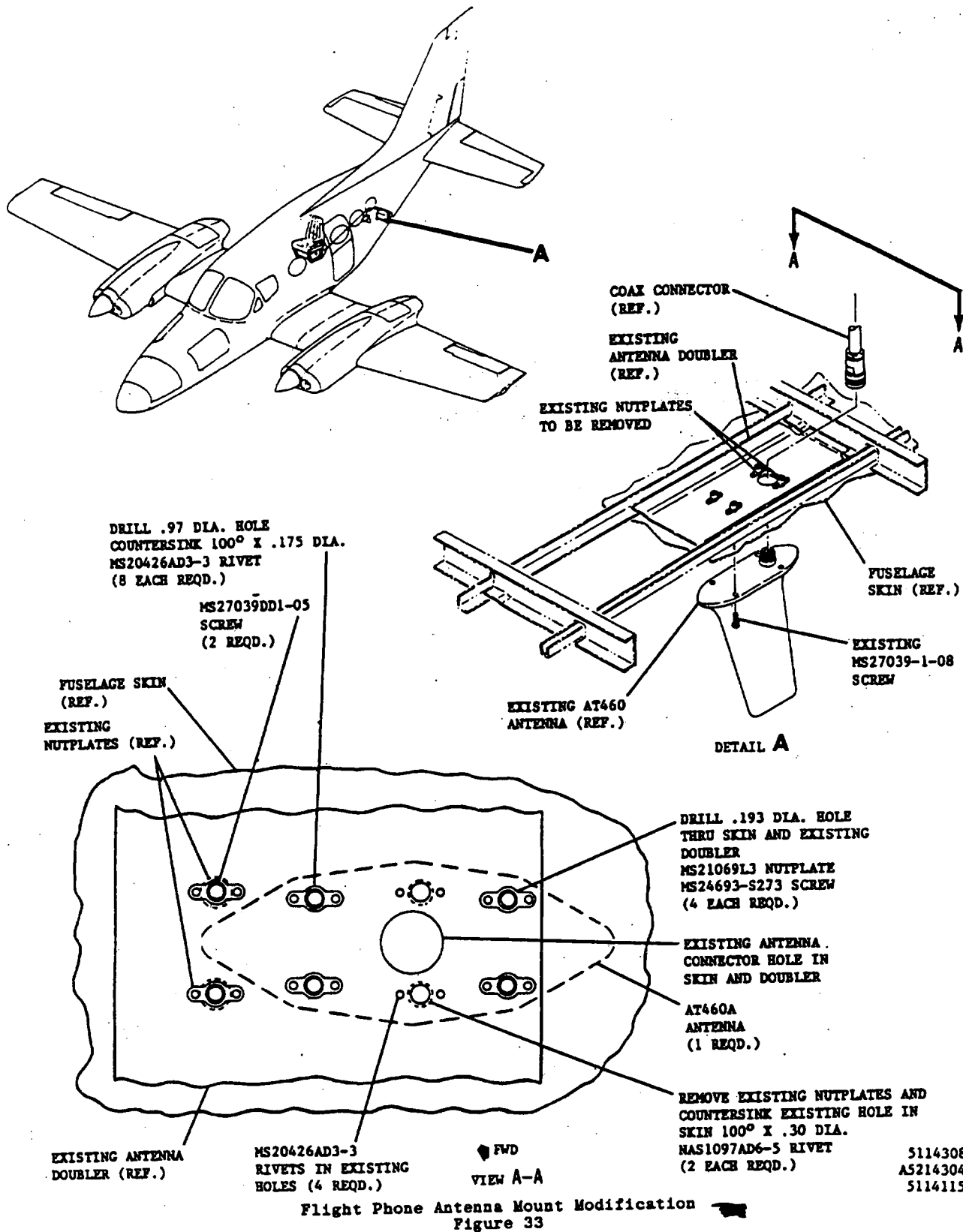


MAJOR FUSELAGE ZONES AND  
FUSELAGE ACCESS PLATES

Ref. 1

Airplane Electrical Zoning  
Figure 1 (Sheet 2)

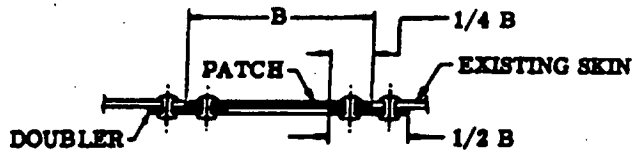
51101005  
51102010



51143087  
 A52143049  
 51141157

Ref. 2

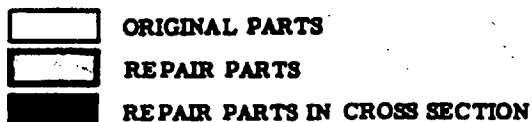
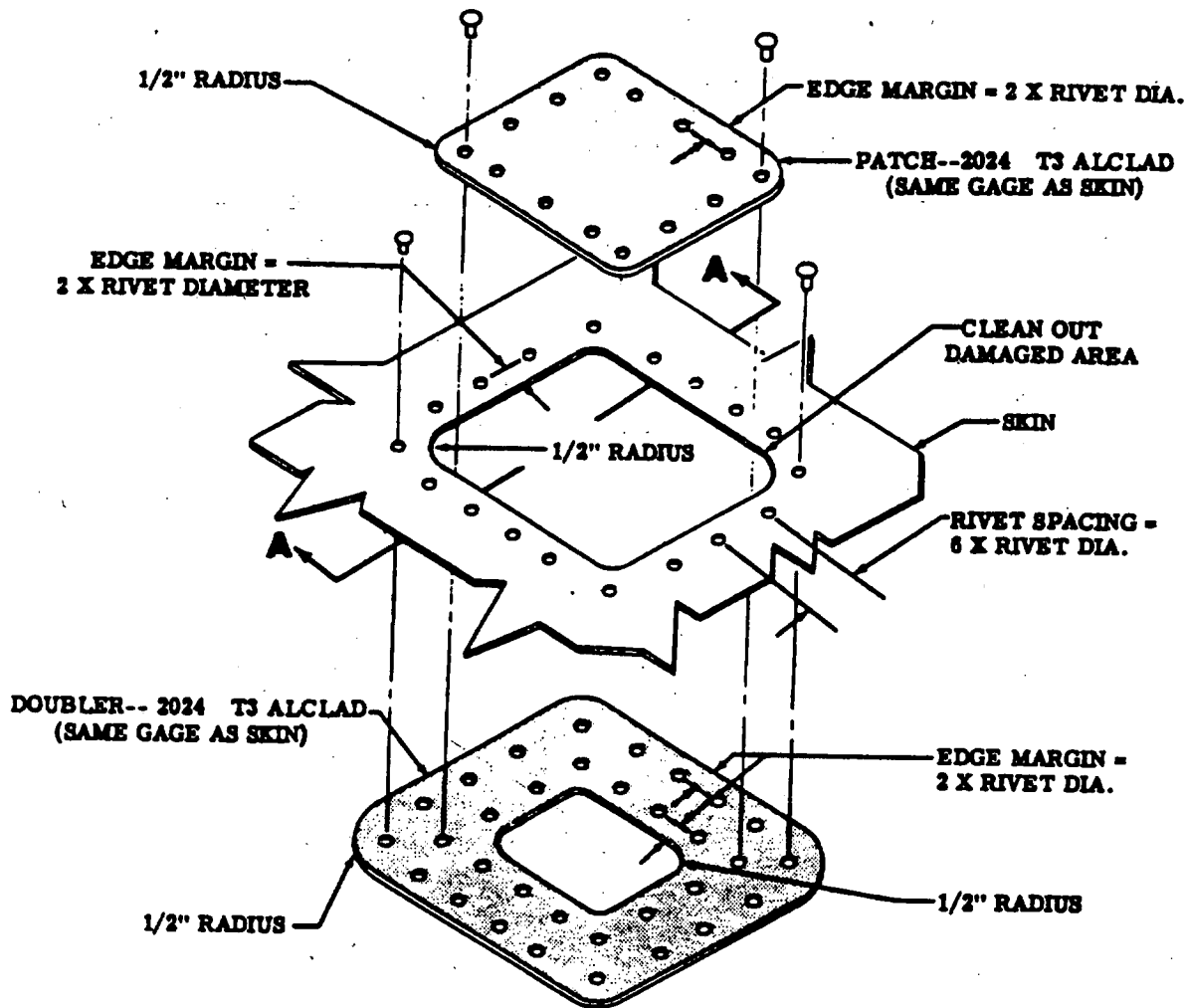




SECTION THRU ASSEMBLED PATCH

A-A

RIVET TABLE	
SKIN GAGE	RIVET DIA.
.020	3/32
.025	1/8
.032	1/8
.040	1/8
.051	5/32



Typical Rectangular Patch in Unpressurized Section  
Figure 13 (Sheet 1 of 2)

Ref. 3

US Department of Transportation  Federal Aviation Administration	<b>MAJOR REPAIR AND ALTERATION</b> <b>(Airframe, Powerplant, Propeller, or Appliance)</b>	Form Approved OMB No. 2120-0020
		For FAA Use Only
		Office Identification <b>ATL F300-11 088</b>

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

1. Aircraft	Make <b>CESSNA</b>	Model <b>421C</b>
	Serial No. <b>421C0897</b>	Nationality and Registration Mark <b>N617CC</b>
2. Owner	Name (As shown on registration certificate) <b>FAIRMONT AVIATION INC</b>	Address (As shown on registration certificate) <b>1013 BROAD RIVER RD #60-13 COLUMBIA, SC 29210-3649</b>

3. For FAA Use Only

4. Unit Identification				5. Type	
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	(As described in item 1 above)				
POWERPLANT	Teledyne Continental Motors	TSIO-520-NB	613083		X
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

6. Conformity Statement

A. Agency's Name and Address	B. Kind of Agency	C. Certificate No.
<b>RAM Aircraft, Limited Partnership P. O. Box 5219 Waco, Texas 76708</b>	<input type="checkbox"/> U. S. Certificated Mechanic	<b>Airframe Class III Powerplant Class I VA1R551K</b>
	<input type="checkbox"/> Foreign Certificated Mechanic	
	<input checked="" type="checkbox"/> Certificated Repair Station	
	<input type="checkbox"/> Manufacturer	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>July 11, 2005</b>	Signature of Authorized Individual <b>Travis D. Watts</b> <i>Travis D. Watts</i>
------------------------------	---

7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is ☒ APPROVED ☐ REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	OTHER (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canada Airworthiness Group	
Date of Approval or Rejection <b>July 11, 2005</b>		Certificate or Designation No. <b>VA1R551K</b>	Signature of Authorized Individual <b>Travis D. Watts</b> <i>Travis D. Watts</i>	

## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Cessna 421C0897 N617CC

Dated 07-11-05 Engine Previous Total: 1309.2

Relocated Turbo Oil Supply Line I/A/W RAM Dwg. No. 1224, Rev. H dated 11/18/03 and installed locknuts on cylinder attachment studs I/A/W Dwg. 1517, Rev. F dated 3/9/05 per STC SE8338SW.

Installation mechanic must complete Block 1 and 2 on reverse side and mail one copy to their local FSDO.

Negligible weight and balance change.

Customer furnished with FAA approved Overhaul and Parts Manual Supplements for all alterations.

Pertinent details of the above installations are on file under Project No. 2182/24219.

-----END-----

☐ Additional Sheets Are Attached

DIVERSIFIED EXECUTIVE ENTERPRISES, LLC  
3655 ROSWELL ROAD, STE 202  
ATLANTA, GA 30342

## FACSIMILE TRANSMITTAL SHEET

TO:	FROM:
Ken Feist	Paul D'Agnese
COMPANY:	DATE:
	3/14/05
FAX NUMBER:	TOTAL NO. OF PAGES INCLUDING COVER:
404-305-7226	1
PHONE NUMBER:	SENDER'S FAX NUMBER:
	404-724-0377
RE:	SENDER'S REFERENCE NUMBER:
	404-724-0669

☐ URGENT    ☒ FOR REVIEW    ☐ PLEASE COMMENT    ☒ PLEASE REPLY    ☐ PLEASE RECYCLE

## NOTES/COMMENTS:

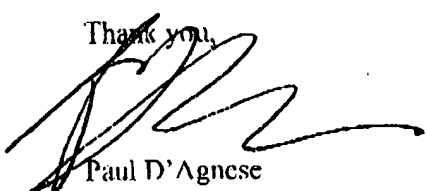
Attn: Ken Feist

From Paul D'Agnese

Ken,

I am requesting a placement certificate as my original certificate has been lost for Cessna 421c-0897, registration #N617CC. The copy of log book showing last annual is being forwarded to you from Epps Aviation. Please feel free to call me on my cell at the number referenced below should you have any additional needs.

Thank you,



Paul D'Agnese  
Title Owner  
404-307-1691

TOTAL	TOTAL	TACH OR	
<b>N617CC</b>	<b>Cessna 421C</b>	<b>421C-0897</b>	<b>ACTT: 5138.1</b>
(Registration Number)	(Aircraft Make and Model)	(Serial Number)	<b>HOBBS: 5138.1</b>
<p><b>The following airframe inspection's were complied with at this time:</b></p> <ul style="list-style-type: none"> <li>Complied with the Annual Inspection on this aircraft, Cessna 421C, S/N 421C-0897, in accordance with Cessna 421C Service Manual, Section's 2-41, 2-42, 2-43, and 2-44.</li> </ul> <p><b>The following Airworthiness Directive's (AD's) were verified or complied with at this time:</b></p> <ul style="list-style-type: none"> <li><b>AD 96-20-07, C/W</b>, by performing heater system pressure decay test. The test was satisfactory at this time. <u>Note:</u> The heater hobbs reading is 1370.4 at this time. The next test is due at 1470.4, or 24 months, whichever occur's first.</li> <li><b>AD 2003-26-14, N/A</b>, due to Liberty brand fire extinguisher being installed.</li> <li><b>AD 2004-08-10, C/W</b>, by verifying this AD is not applicable to the part number that's installed on the aircraft.</li> </ul> <p><b>The following airframe maintenance tasks' were accomplished at this time:</b></p> <ul style="list-style-type: none"> <li>Re-rigged the elevator control system, in accordance with Cessna 421C Service Manual, Section 5-30, Page 2.</li> <li>Adjusted the elevator trim tab cable tension as required, IAW Cessna 421C SM, Section 5-30, Page 3, Paragraph (E), and Section 1-00, Page 2, Paragraph (H).</li> <li>Performed an elevator trim tab travel check, IAW Cessna 421C SM, Section 5-30, Page 3, and Section 1-00, Page 2. Travel check was satisfactory, and found to be correct at this time.</li> <li>Removed the existing unserviceable Emergency Locator Transmitter (ELT) Battery, and installed a serviceable (new) Artex ELT Battery Pack, P/N 00-60-62, IAW Artex Battery Replacement Instructions 571-0007, Rev. A, DCN 1540 (11-28-00). <u>Note:</u> The next battery replacement date is April 2006.</li> <li>Repaired a chaffed power wire for the tail navigation light, by splicing the wire with an environmental splice, P/N D436-36, and repositioning the wire with a cable tie and stand off to prevent further chaffing, IAW AC43.13-1B, Para. 11-167(b).</li> <li>Installed anti-chaff tape, P/N 09-30450, on the LH alternate static air cable, in the center pedestal, IAW AC 43.13-1B, Para. 7-149 (j).</li> <li>Cleaned and lubricated the elevator trim tab control chain and cable, in the center pedestal, IAW AC 43.13-1B, Para. 7-152.</li> <li>Installed an alligator chaff strip, P/N MS21266-39, on a rib located in the LH wing panel, at wing station 58.94, to prevent further line chaffing, IAW AC 43.13-1B, Para. 7-149(j).</li> <li>Removed and reinstalled the RH engine tachometer generator heat shield as required to facilitate the welding repair process, IAW AC 43.13-1B, Section 4-75, Part D, and Cessna 421C SM, Section 7-80, Page 1, Para. 1 (D)(2).</li> <li>Removed the existing unserviceable negative cable lug from the main aircraft battery, and installed a serviceable (new) AWG #4 cable lug onto the negative cable for the main aircraft battery, IAW AC 43.13-1B (Para. 11-178).</li> <li>Removed the existing unserviceable (broken) ground wire terminal on the LH engine heater, at the #4 cylinder valve cover area, and installed a serviceable (new) ring terminal, P/N MS25036-154, IAW AC 43.13-1B, Para. 11-177.</li> <li>Removed and replaced the RH wing locker compartment light bulb (1ea.), P/N 1309, IAW Cessna 421C SM, Section 13-32, Page 6.</li> <li>Removed and reinstalled the LH elevator assembly, to facilitate the installation of the inboard bonding strap (1ea.), P/N FB-1/4", IAW Cessna 421C SM, Section 5-30, Page 8, and AC 43.13-1B, Section 11-186, Paragraph b.</li> <li>Adjusted the LH and RH aileron travel stop's in the LH and RH wing area's as required, IAW Cessna 421C SM, Section 5-10, Page 5.</li> <li>Removed and replaced the cabin light bulb's (5ea.), P/N 1495, located on the LH side of the cabin area, as required. Performed an operational check on the light bulb's. No defect's noted at this time.</li> <li>Serviced the pilot's LH brake pedal master cylinder with MIL-HI-5606 hydraulic fluid, IAW Cessna 421C SM, Section 2-60.</li> <li>Removed and replaced the cockpit manifold pressure gauge post light bulb's (2ea.), P/N 6240-99-995-9118, IAW Cessna 421C SM, Section 13-31, Page 1. Operational check was satisfactory at this time.</li> <li>Installed serviceable (new) hose clamp's (2ea.), P/N QS200M-44, on the LH air conditioner evaporator vent blower and duct, located under the floor board, below the pilot's seat.</li> <li>Removed the existing unserviceable oxygen cylinder, P/N DOT-3HT, S/N 53518, with regulator, P/N 172010-01, and installed a serviceable (overhauled) oxygen cylinder (1ea.), P/N 176018-115, S/N 790764, Mfg. Date 12-03, with a serviceable (overhauled) regulator (1ea.), P/N 172010-41, S/N C10363, IAW Cessna 421C SM, Section 6-11, Pages 1 and 5. <u>Note:</u> The oxygen cylinder and regulator was received serviceable from AVOX, (W.O. 090226).</li> <li>Primed a chaffed area on the inner surface skin of the RH engine cowling 420DT, as required to prevent corrosion.</li> <li>Repaired corrosion around the tear drop panel, located aft of the vent on the lower fuselage surface, at fuselage station 305.94, by removing the surface corrosion, applying alodine to the bare surface's, and painting epoxy primer over the entire surface area.</li> <li>Adjusted the aileron control cable tension to 25 lbs., and performed a travel check as required, IAW Cessna 421C SM, Section 5-10, Pg. 4.</li> <li>Removed and replaced the cabin entrance threshold floorboard panel (1ea.), P/N 5111226-31.</li> <li>Removed and replaced the nose landing gear uplock roller (1ea.), P/N NAS43HT15-108, and spacer (1ea.), P/N NAS43HT3-111, IAW Cessna 421C SM, Section 4-33.</li> <li>Removed the existing unserviceable RH fuel selector gear box, and installed a serviceable (new) fuel selector gear box (1ea.), P/N C100098-6, IAW Cessna 421C SM, Section 9-10, Page 6. Functional check was satisfactory.</li> <li>Installed the upper cabin door strut safety clip (1ea.), P/N A9002, IAW Cessna 421C SM, Section 3-11, Page 11.</li> <li>Removed and replaced the LH main landing gear uplock roller (1ea.), P/N 5741157-1, and bolt (1ea.), P/N NAS6204-23D, IAW Cessna 421C SM, Section 4-11.</li> <li>Removed and replaced the RH main landing gear uplock roller (1ea.), P/N 5741157-1, and bolt (1ea.), P/N NAS6204-23D, IAW Cessna 421C SM, Section 4-11.</li> <li>Removed minor corrosion (rust) from inside of the forward cavity of the LH and RH main landing gear trailing link assemblies, and treated the affected area's, as required, IAW Cessna 421C SM, Section 1-80.</li> <li>Removed and replaced the LH main landing gear trailing link assembly bearing's (2ea.), P/N M81934/2-24A022, and washer's (2ea.), P/N S3313-1, IAW Cessna 421C SM, Section 4-11, Page 3.</li> <li>Removed and replaced the LH and RH main wheel assembly keyed washer's (2ea.), P/N MS21258C15, IAW Cessna 421C SM, Section 4-11, Page 1.</li> <li>Repaired a broke wire (1143B20), on the back of the cabin pressurization switch, by soldering the wire back in place, and installing heat shrink tubing for insulation.</li> </ul>			
PAGE 1 OF 4			

SUB-TOTALS this page

TOTALS—Carry forward to next page



	TOTAL	TOTAL	TACH OR
<ul style="list-style-type: none"> <li>Removed all of the existing unserviceable aileron, elevator, and rudder static discharger wick's and base's, and installed serviceable (new) static discharger wick's (8ca.), P/N DD1W, and base's (8ca.), P/N 5900012-1-1. IAW Cessna 421C SM, Section 14-90, Page 1, Step's A thru D.</li> <li>The aircraft IFR recertification was complied with at this time, by Northwest Arkansas Avionics, inc. (W.O. 0205). <u>Note:</u> For pertinent detail's of the IFR certification, see NWAA logbook entry, dated 4/22/04.</li> <li>Performed a compass swing on the aircraft standby compass, as required.</li> <li>Installed a PM Research radome boot (PM-56) onto the nose radome assembly after the paint repair process, IAW PM Research Installation Instructions.</li> <li>Removed the existing unserviceable stall warning lift detector, P/N 186-9, S/N 1765, and installed a serviceable (overhauled) stall warning lift detector (1ea.), P/N 186-8, S/N 1112. IAW Cessna 421C SM, Section 13-40, Page 1. <u>Note:</u> The installed lift detector was received in a serviceable (overhauled) condition from Safe Flight Instrument Corporation (W.O. 337400).</li> </ul> <p>The following airframe structural repair tasks were accomplished at this time:</p> <ul style="list-style-type: none"> <li>Repaired a small hole on the fuselage surface, forward of the RH nose landing gear door, at fuselage station 31.00, by removing the damaged area, and installing a 2024-T3 filler repair patch (1.0"x1.0"x.025), with a 2024-T3 internal doubler (3.0"x3.0"x.032), IAW Cessna 421C SM, Section 15-40, Page 13, Figure 10.</li> <li>Repaired the working rivet's on the upper aft surface of the LH engine nacelle, by removing and installing the following serviceable (new) rivet's: P/N's MS20470AD-4, NAS1097AD-4, and CR3243-4, IAW Cessna 421C SM, Section 15-00, as required.</li> <li>Repaired a cracked cover panel (#14), located in the nose compartment, by removing panel, duplicating a new panel out of 2024-T3 material (1.0"x1.5"x.020), and installing in nose compartment, IAW Cessna 421C SM, Section 15-00.</li> <li>Repaired minor surface corrosion around the vent on the lower fuselage surface, at fuselage station 289.94, by removing the corrosion, applying alodine to the bare surfaces, and painting the entire area with epoxy primer. IAW Cessna 421C SM, Section 15-50, Page 6, Para. C.</li> <li>Repaired minor surface corrosion around the tear drop panel on the lower fuselage surface aft of the vent, at fuselage station 305.94, by removing the corrosion, applying alodine to the bare surfaces, and painting the entire area with epoxy primer. IAW Cessna 421C SM, Section 15-50, Page 6, Para. C.</li> <li>Repaired chaffing and a crack on the lower RH wing skin, located forward of the RH MLG forward trunnion bolt, by removing the damaged area, and installing a 2024-T3 .032 doubler repair patch over the affected area, using EA9309 adhesive and rivet's, IAW Cessna 421C SM, Section 15-40, Page 39, Para. C.</li> <li>Repaired chaffing and a crack on the lower LH wing skin, located forward of the LH MLG forward trunnion bolt, by removing the damaged area, and installing a 2024-T3 .040 doubler repair with a 2024-T3 .032 filler, bonded together and to the aircraft using EA9309 adhesive and rivet's. IAW Cessna 421C SM, Section 15-40, Page 39, Para. C.</li> <li>Removed the working rivet's on the LH inboard flap skin, and installed serviceable (new) rivet's, on the LH inboard flap skin. IAW Cessna 421C SM, Section 15-00, Page 3.</li> <li>Repaired minor erosion on the wing leading edge skin, outboard of the stall warning vane, by burnishing the metal to contour. IAW Cessna 421C SM, Section 15-00.</li> <li>Repaired a crack on the floorboard skin, located forward of the pilot's rudder pedal's, where the alternate static air cable's route through, by removing the damaged skin, and installing a 2024-T3 doubler repair patch (7"x2"), over the affected area, using CR 3243 4-3 rivet's, IAW Cessna 421C SM, Section 15-40.</li> <li>Removed the working rivet's on the surface area located forward of the RH main landing gear wheel well, and installed serviceable (new) rivet's (6ea.), P/N CR3212-4, IAW Cessna 421C SM, Section 15-00.</li> <li>Removed the working rivet's on the LH and RH wing to fuselage lower fairing's, cleaned and resealed the area's, and installed serviceable (new) rivet's (as required). P/N's NAS1097AD-5, MS20426AD-4, and CR3242-4, IAW Cessna 421C SM, Section 15-00 and Section 1-80.</li> <li>Repaired a crack in the aft fuselage tailcone assembly, IAW Cessna 421C SM, Section 15-40, Page 1.</li> <li>Removed and replaced nutplate's (2ca.), P/N MS210759L3, under engine nacelle panel #621AT, IAW Cessna 421C SM, Section 15-00.</li> <li>Repaired a chaffed forward stiffener, located at 410DT, on the LH engine cowl, by installing (2ca.) stainless steel doubler repair's, (1"x5"x.020), at the affected area, using Monel double flush rivet's and EA9309 epoxy adhesive, IAW Cessna 421C SM, Section 15-40.</li> <li>Repaired a chaffed aft stiffener, located at 410DT, on the LH engine cowl, by blending out the wear damage and corrosion, and installing (3ea.) .020 stainless steel dimpled internal doubler repair's, (1 1/8"x15"), (1 1/8"x12"), and (1 1/8"x15"), using Monel double flush rivet's (3-3), NAS1097AD4-3 rivet's, and EA9309 epoxy adhesive, IAW Cessna 421C SM, Section 15-40.</li> <li>Installed (4ea.) .020 stainless steel chaff plate's, (2"x12"), (2"x13"), (2"x6"), and (1 1/8"x14") over the affected area's of the LH engine cowl at location 410DT, using NAS1097-3-3 rivet's and EA9309 epoxy adhesive, IAW Cessna 421C SM, Section 15-40.</li> <li>Installed a rivet into the support angle to frame, under the floorboard, next to the ventilation duct, at fuselage station 200.75, IAW Cessna 421C SM, Section 15-00.</li> <li>Repaired the disbonded tip on the nose radome assembly, by removing the erosion boot, and applying a repair to the affected area. IAW Cessna 421C SM, Section 15-40, Page 3.</li> <li>Repaired a crack on the upper composite section of the RH main landing gear door assembly, by stop drilling the crack on both end's, cleaning out the length of the crack, filling all honeycomb void's using epoxy adhesive EA960F, and applying fiberglass overlay using the same epoxy adhesive, IAW Cessna 421C SM, Section 15-10, Page 10.</li> <li>Repaired the damaged skin around the lighter assembly in the cargo area, by removing the damaged skin area, and installing a 2024-T3 .032 aluminum doubler and filler repair patch over the affected area, IAW Cessna 421C SM, Section 15-40, Page 39.</li> <li>Repaired the double row of loose rivet's around the wing panel #632AB, outboard of the gear door, by removing the double row of rivet's, cleaning the sealant around the hole's, and installing serviceable (new) rivet's P/N NAS 1097AD-6-5, IAW Cessna 421C SM, Section 15-00. Sealed the repaired area. IAW Cessna 421C SM, Section 1-80.</li> <li>Repaired a row of loose rivet's on the LH nose door hinge assembly, by gaining access under the nose baggage compartment floor, removing the loose rivet's, and replacing with AD solid rivet's (14ea.), IAW Cessna 421C SM, Section 15-00.</li> <li>Repaired several loose rivet's on the upper surface of the RH wing, between the fuselage and engine nacelle, by removing the loose rivet's, and installing serviceable (new) rivet's, P/N's CR3214-5 and CR3243-4, IAW Cessna 421C SM, Section 15-00.</li> </ul>			

SUB-TOTALS this page

TOTALS—Carry forward to next page

TOTAL	TOTAL	TACH OR																
<p>Repaired the loose nose landing gear upper LH trunnion mount assembly, by replacing the shim's (2ea.), P/N's 5013049-1 and 5013049-4, on the nose wheel well LH and RH keelson's, installing stainless steel plate (approx. 3"x5"x.035) on the other side of the keelson, using AD-5 rivet's and epoxy adhesive FA9309, IAW Cessna 421C SM, Section 5-40.</p> <p>Repaired a chaffed LH wing lower forward spar cap, located above the LH main landing gear door assembly, by removing the existing unserviceable spar cap, duplicating a new spar cap from 2024-F3 .063 aluminum, and installing on the aircraft, using Hi-Lok fastener's and rivet's, IAW Aerodesign Aircraft Engineering Report No. 5054-1, IR for fastener substitution. Note: Adjusted the LH MLG door assembly as required to prevent further chaffing. Performed a landing gear operational check as required. The operational check was satisfactory at this time.</p> <p>The following airframe alteration was accomplished at this time:</p> <ul style="list-style-type: none"> <li>Installed 23ea. HL19PB6 fastener's and HL70 collar's, in the place of 23ea. NAS1055-6 fastener's in the LH lower forward spar, between wing station 159.00 and wing station 190.00, in accordance with Rocky Howard Engineering Report No. 5054-1, Revision IR, and documented with FAA Form 8110-3, Dated April 20, 2004.</li> <li>The aircraft weight change is negligible, and the aircraft weight and balance is not affected.</li> <li>For pertinent detail's of the above alteration, see Rocky Howard Engineering Report No. 5054-1, Revision IR, and Rose Aircraft Services, Inc. FAA Form 337, Dated 04-21-2004.</li> </ul> <p>All airframe inspection's, maintenance tasks', structural repair tasks', and airframe alteration's were performed in accordance with Cessna 421C Service Manual, Illustrated Parts Catalog, Rocky Howard Engineering Report No. 5054-1, Rev. IR, and AC 43.13-1B specification's.....END.....</p> <p>The aircraft, airframe, or appliance identified above was repaired and inspected in accordance with current regulations of the Federal Aviation Administration and is approved for return to service. Pertinent details of the repairs are on file at this repair station under Work Order Number <u>9653</u>.</p> <p>Date <u>April 23, 2004</u> Signed <u>Bruce A. Myers</u> <u>Bruce A. Myers</u> (Authorized Signature For)</p> <p><b>ROSE AIRCRAFT SERVICES, INC.</b> <b>CRS-HR2R882K</b> <b>INTERMOUNTAIN REGIONAL AIRPORT</b> <b>P.O. BOX 1363 * MENA, ARKANSAS 71953</b></p>																		
<p>HILL AIRCRAFT &amp; LRAS FAA Certified Repair Station ESER686D</p> <p>Customer Name: American Express</p> <table border="1"> <thead> <tr> <th>Shop</th> <th>Work Order</th> <th>Reg.No.</th> <th>Make</th> <th>Model</th> <th>Serial</th> <th>A/C TT</th> <th>Closed</th> </tr> </thead> <tbody> <tr> <td>005</td> <td>6873</td> <td>N617CC</td> <td>CESSNA</td> <td>421C</td> <td>421C0897</td> <td>5166.20</td> <td>JUL 7 04</td> </tr> </tbody> </table> <p>OPR Description Page 1</p> <p>1 Removed and reinstalled RH brake back plates using new hardware. Removed and reinstalled LH brake back plates with new linings, P/N 066-06600, 1 each new back plate, P/N 074-03400, new attachment bolts, P/N 103-11800 and washers P/N 095-10200. Above work performed IAW C421C Service Manual, 4-43.</p> <p>959 ***** THE AIRCRAFT, AIRFRAME, AIRCRAFT ENGINE, PROPELLER OR APPLIANCE (IDENTIFIED ABOVE WAS REPAIRED AND INSPECTED IN ACCORDANCE WITH CURRENT REGULATIONS OF THE FEDERAL AVIATION ADMINISTRATION AND IS APPROVED FOR RETURN TO SERVICE. SIGNED: <u>David Thompson</u> FOR CRS# ESER686D *****</p>			Shop	Work Order	Reg.No.	Make	Model	Serial	A/C TT	Closed	005	6873	N617CC	CESSNA	421C	421C0897	5166.20	JUL 7 04
Shop	Work Order	Reg.No.	Make	Model	Serial	A/C TT	Closed											
005	6873	N617CC	CESSNA	421C	421C0897	5166.20	JUL 7 04											
<p>SUB-TOTALS this page</p> <p>TOTALS—Carry forward to next page</p>																		





U.S. Department of  
Transportation  
Federal Aviation  
Administration

**MAJOR REPAIR AND ALTERATION**  
**(Airframe, Powerplant, Propeller, or Appliance)**

Form Approved -  
OMB No. 2120-0020

For FAA Use Only

Office Identification

ABW-FSDO-11

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act 1958).

1. Aircraft	Make CESSNA	Model 421C
	Serial No. 421C0897	Nationality and Registration Mark N517CC
2. Owner	Name (As shown on registration certificate) COB CAPITAL CORP	Address (As shown on registration certificate) 6065 ROSWELL RD NE # 2400 ATLANTA GA 30328-4011

3. For FAA Use Only

4. Unit Identification				5. Type	
Unit	Make	*Model	Serial No.	Repair	Alteration
AIRFRAME	(As described in item 1 above)				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

6. Conformity Statement

A. Agency's Name and Address	B. Kind of Agency	C. Certificate No.
ROSE AIRCRAFT SERVICES, INC. 132 FLIGHT LN. MENA AR 71953	<input type="checkbox"/> U.S. Certified Mechanic <input type="checkbox"/> Foreign Certified Mechanic <input checked="" type="checkbox"/> Certified Repair Station <input type="checkbox"/> Manufacturer	HR2R882K

I, D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date 04-21-2004	Signature of Authorized Individual R. J. BAKER
--------------------	---

7. Approval for Return to Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is ☒ APPROVED ☐ REJECTED

BY	FAA Fit Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee	X Repair Station	Person Approved by Transport Canada Airworthiness Group	
Date of Approval or Rejection 04-21-2004	Certificate or Designation No. HR2R882K	Signature of Authorized Individual R. J. BAKER		

## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

CESSNA, 421C S/N 421C0897 DATE: 04-21-2004.

1. INSTALLED 23ea HL19PB6 FASTENERS AND HL70 COLLARS IN PLACE OF 23ea NAS1055-6 FASTENERS IN THE LEFT LOWER FORWARD SPAR, BETWEEN W.S. 159 AND 190 IN ACCORDANCE WITH ROCKY HOWARD ENGINEERING REPORT NO. 5054-1 REV. IR AND DOCUMENTED WITH FAA FORM 8110-3 DATED APRIL 20, 2004.
2. WEIGHT CHANGE IS NEGLIGIBLE, AIRCRAFT WEIGHT AND BALANCE IS NOT AFFECTED.

END

RECEIVED

APR 23 2004

LIT-FSDO-11

X Additional Sheets Are Attached

0404-58

Date

APR 20 2004

U.S. Department of Transportation

Federal Aviation Administration

## STATEMENT OF COMPLIANCE WITH THE FEDERAL AVIATION REGULATIONS

Aircraft or Aircraft Component Identification

Note	Model No.	Type (Airplane, Rotor, Helicopter, etc.)	Name of Applicant
CESSNA AIRCRAFT	421C	AIRPLANE	ROSE AIRCRAFT SERVICE

## LIST OF DATA

Identification	Title
AERODESIGN AIRCRAFT INC., INC.	
REPORT #0054-1, REV. IR, 4-20-04	STRUCTURAL SUBSTANTIATION, FASTENER SUBSTITUTION FOR REPAIR - CESSNA MODEL 421C AIRCRAFT
STRUCTURAL APPROVAL ONLY	
only for Cessna Model 421C, S/N 421C0297, NS170C	

## Purpose of Data

SUPPORT OF MAJOR REPAIR, APPROVAL ONLY FOR DESIGN OF INSTALLATION

## Applicable Requirements (List specific sections)

CAR 3.171(a)(4), 3.172, 3.173, 3.174, 3.151, 3.155, 3.123, 3.222, 3.223, 3.294,  
3.295, 3.296, 3.301, 3.303, 3.307

CERTIFICATION - under authority vested by direction of the Administrator and in accordance with limitations of appointment under Part 183 of the Federal Aviation Regulations, data listed above and on attached sheets numbered \_\_\_\_\_ (none) have been examined in accordance with established procedures and found to comply with applicable requirements of the Federal Aviation Regulations.

☐ Recommend approval of these data

I (We) Therefore

☒ Approve these data

Signature(s) of Designated Engineering Representative(s)

Designation Number(s)

Certification

R.M. Howard, Jr.

CERT-710134-SW

Signature

US Department of Transportation  Federal Aviation Administration	<b>MAJOR REPAIR AND ALTERATION</b> <b>(Airframe, Powerplant, Propeller, or Appliance)</b>	Form Approved OMB No. 2120-0020
		For FAA Use Only <i>FAA</i>
		Office Identification <i>AFW FSDO</i>

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

1. Aircraft	Make  Cessna	Model  421C
	Serial No.  421C0897	Nationality and Registration Mark N6174C
2. Owner	NAME (As shown on registration certificate)  Cobb Capital Corp.	Address (As shown on registration certificate)  6065 Roswell Rd NE # 2400 Atlanta, GA 30328-4911 <b>RECEIVED</b>

For FAA Use Only

MAR 19 2002

4. Unit Identification				5. Type	
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	(As described in item 1 above)				X
POWERPLANT	Teledyne Continental Motors	GTSIO-520-H	L-808297-R R-808298-R		X
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

#### 6. Conformity Statement

A. Agency's Name and Address	B. Kind of Agency	C. Certificate No.
Ram Aircraft, Limited Partnership P. O. Box 5219 Waco, Texas 76708	<input type="checkbox"/> U. S. Certificated Mechanic	AIRFRAME CLASS III POWERPLANT CLASS I VA1R551K
	<input type="checkbox"/> Foreign Certificated Mechanic	
	<input checked="" type="checkbox"/> Certificated Repair Station	
	<input type="checkbox"/> Manufacturer	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date  January 7, 2002	Signature of Authorized Individual  Stephen Wade <i>S. Wade</i>
-----------------------------	---

#### 7. Approval for Return To Service

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is ☒ APPROVED ☐ REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	OTHER (Specify)
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canada Airworthiness Group	

Date of Approval or Rejection  January 7, 2002	Certificate or Designation No.  VA1R551K	Signature of Authorized Individual  Stephen Wade <i>S. Wade</i>
--	--	---

**NOTICE**

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

**8. Description of Work Accomplished**

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

**Cessna 421C0897 N6174C**  
**Airframe Total Time: 4473.4 Dated 02-07-02**

Engine crankcases modified per Dwg. 1514, Rev. H dated 2/2/99 I/A/W STC SE8338SW.

Relocated turbo oil supply lines I/A/W RAM Dwg. No. 1224, Rev. F, dated 2/2/99 and installed locknuts on cylinder attached studs I/A/W Dwg. 1517, Rev. C dated 6/8/98 per STC SE8338SW.

Installed Power Pac Spoilers kit SP400 per STC SA4913NM

Inspected previously installed A.T.S V.G's per installation manual IM92-6A. Inserted Flight manual Supplement ATS 93-02a in aircraft P.O.H. Installed placard P/N 1C0067000-02 on instrument panel per STC SA00015SE.

Installed Floscan Fuel Flow transducers per Dwg. 1084, Rev. N, dated 10/20/00 in accordance with STC SE5726SW.

Installed Shadin Fuel Flow Indicating System; per Dwg. 1084, Rev. N, dated 10/20/00 I/A/W STC SA5796SW.

Installed range-marked EGT, CHT, oil temperature and pressure gages I/A/W Dwg. 1152, Rev. B dated 8/27/97 per STC SA5878SW. Flight manual Supplement dated 2/1/85 furnished.

Installed Alcor EGT system per manufacturer's instructions I/A/W STC SA522SW.

Installed improved engine cooling baffles and improved exhaust system slip joints per Dwg's. 1009, Rev. J, dated 11/5/96 and 1001, Rev. U dated 04/06/01 I/A/W STC SA4592SW.

Installed spring loaded induction hose clamps per Dwg. 1171 Rev. B, dated 5/24/00 I/A/W STC SE3767SW.

Installed new NDM oil cooler P/N CAM 646685 I/A/W STC SE00577NY.

Installed vacuum pump cooling shroud on left and right engine vacuum pumps I/A/W RAM Dwg. 1221, Rev. K, dated 10/27/98 and RAM Dwg. 1199, Rev. H, dated 9/30/96 per STC SA3721SW.

New empty weight and balance computed.

Customer furnished with FAA approved Overhaul and Parts Manual Supplements for all alterations.

Customer furnished with FAA approved Flight Manual Supplements for all operations.

Pertinent details of the above installations are on file under work order no. 5450.

---End---



US Department  
of Transportation  
Federal Aviation  
Administration

**MAJOR REPAIR AND ALTERATION**  
**(Airframe, Powerplant, Propeller, or Appliance)**

Form Approved  
OMB No. 2120-0020

For FAA Use Only

Office Identification

**GL-11 / IND FSDO**

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

1. Aircraft	Make <i>CESSNA</i>	Model <i>421C</i>
	Serial No. <i>421C0897</i>	Nationality and Registration Mark <i>N 6174C</i>
2. Owner	Name (As shown on registration certificate) <i>TREXAWAY AVIATION INC.</i>	Address (As shown on registration certificate) <i>7406 PROMONTORY CIR INDIANAPOLIS, IN. 46236</i>

**3. For FAA Use Only**

4. Unit Identification				5. Type	
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	<i>~~~~~ (As described in Item 1 above) ~~~~~</i>			<input checked="" type="checkbox"/>	
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

**6. Conformity Statement**

A. Agency's Name and Address <i>Mount Comfort Airport 3367 Aviation Way Greenfield, IN 46140</i>	B. Kind of Agency <input checked="" type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer	C. Certificate No. <i>306024850 AWP</i>
---	--	--

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <i>20 APR 01</i>	Signature of Authorized Individual <i>[Signature]</i>
--------------------------	--

**7. Approval for Return To Service**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is ☒ APPROVED ☐ REJECTED

BY	FAA Flt. Standards Inspector	Manufacturer	<input checked="" type="checkbox"/> Inspection Authorization	Other (Specify)
	FAA Designee	Repair Station	Person Approved by Transport Canada Airworthiness Group	
Date of Approval or Rejection <i>04/20/01</i>		Certificate or Designation No. <i>301449288 IA</i>	Signature of Authorized Individual <i>[Signature]</i>	

### NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

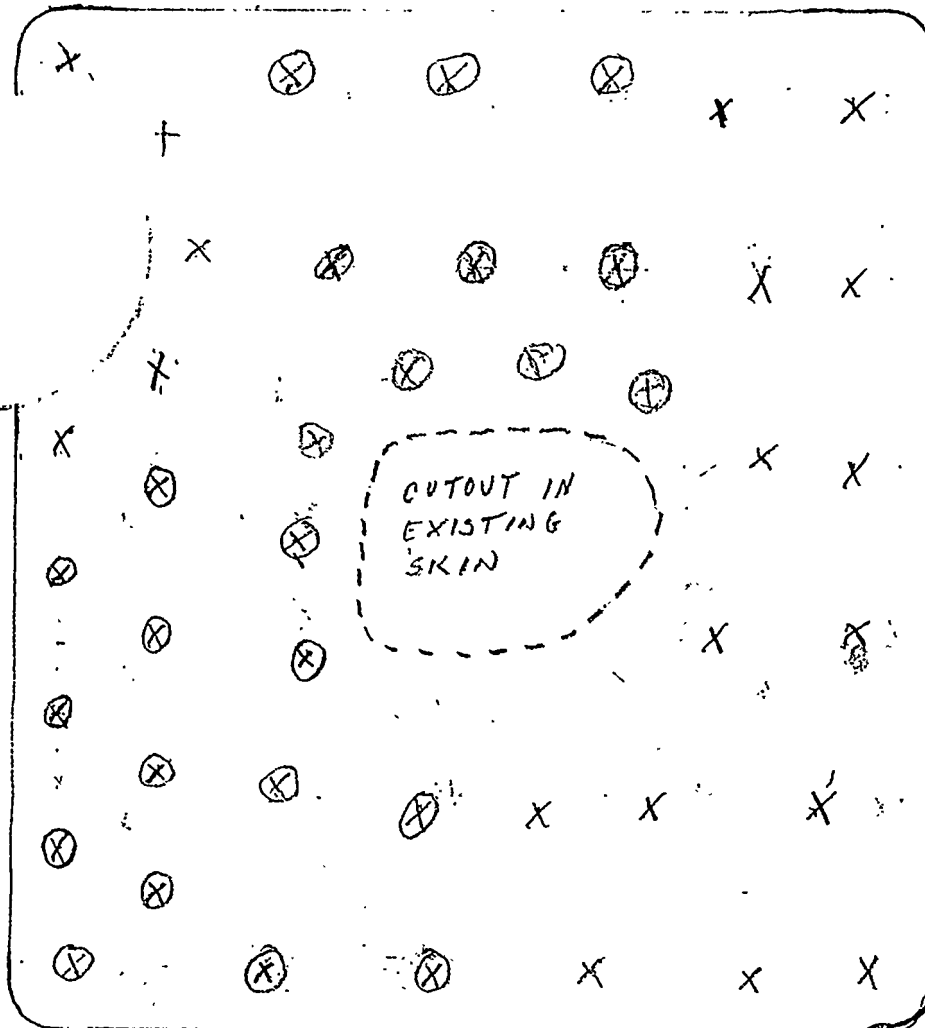
#### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

RIGHT LOWER WING SKIN CORRODED THROUGH AT  
WS 126.00, F.S. 160.00, REMOVED CORRODED AREA,  
APPROX. 1.5" x 1.25". TREATED AFFECTED AREA WITH  
ALODINE AND PRIMED, FABRICATED PATCH USING  
8024 T3 1041" PICKED UP EXISTING RIVETS WHERE  
POSSIBLE, WHERE ADDITIONAL RIVETS WERE NEEDED  
USED MS20426 AD4. RIVETS WERE INSTALLED WBT  
AND WING INTERNALLY SEALED AS REQUIRED. ALL  
WORK PERFORMED IAW CESSNA 421C MAINT. MAN.  
CHAPTER 15, "STRUCTURAL REPAIR" USING TECHNIQUES  
AND STANDARDS PRESCRIBED BY AC 43.13-1B. WT.  
AND BAL. CHANGE NEGLIGIBLE, SEE ATTACHED  
DRAWING. ~~DATE~~

☒ Additional Sheets Are Attached

↑  
FWD  
← OUTBD



X = EXISTING RIVETS  
⊗ = ADDITIONAL RIVETS

EXISTING →  
SKIN LAP





US Department  
of Transportation  
Federal Aviation  
Administration

**MAJOR REPAIR AND ALTERATION**  
**(Airframe, Powerplant, Propeller, or Appliance)**

Form Approved  
OMB No. 2120-0020

For FAA Use Only *6-11*

Office Identification  
**GL-11 / IND FSDO**

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

1. Aircraft	Make <b>CESSNA</b>	Model <b>421C</b>
	Serial No. <b>421C-0897</b>	Nationality and Registration Mark <b>N 6174C</b>
2. Owner	Name (As shown on registration certificate) <b>TREADWAY AVIATION INC</b>	Address (As shown on registration certificate) <b>9406 PROMOTORY CIRCLE INDIANAPOLIS IN 46236</b>

**3. For FAA Use Only**

"The data identified herein complies with the applicable airworthiness requirements and is approved for the above described aircraft, subject to conformity inspection by a person authorized in FAR 43, section 43.7.

Date: **FEB 16 1996** FAA Inspector AGL-IND-FSDO *[Signature]*

4. Unit Identification				5. Type	
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	<i>~~~~~ (As described in Item 1 above) ~~~~~</i>				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				

**6. Conformity Statement**

A. Agency's Name and Address	B. Kind of Agency	C. Certificate No.
<b>Muncie Aviation Co. Box 1169, Muncie Airport Muncie, Indiana 47308</b>	<input type="checkbox"/> U.S. Certificated Mechanic	<b>AJVR135C</b>
	<input type="checkbox"/> Foreign Certificated Mechanic	
	<input checked="" type="checkbox"/> Certificated Repair Station	
	<input type="checkbox"/> Manufacturer	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>2-14-96</b>	Signature of Authorized Individual <b>Kenneth K. Talhelm</b> <i>[Signature]</i>
------------------------	--

**7. Approval for Return To Service**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is ☒ APPROVED ☐ REJECTED

BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee <i>[Signature]</i>	Repair Station	Person Approved by Transport Canada Airworthiness Group	
Date of Approval or Rejection <b>2-21-96</b>		Certificate or Designation No. <b>AJVR135C</b>	Signature of Authorized Individual <b>Kenneth K. Talhelm</b> <i>[Signature]</i>	

### NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

#### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N 6174C: Date 2-14-96

Certified previously installed Trimble TNL 2000 Approach GPS system for IFR enroute flight.

This units operational revision status is NAV 2.573, GPS 4.02-1214.

The aircraft was test flown and found to meet the requirements for IFR enroute and terminal navigation per AC 20-138.

Test Pilot signature certifying IFR accuracy requirements.

  
KENNETH K. TALHELM

#2406901

Date: 2-14-96

A revised TNL 2000 Approach Pilots guide, P/N 81449 rev E dated April 14, 1995 or later version, has been placed in the aircraft.

The FAA Approved Flight Manual Supplement accompanying this Field Approval is issued to this aircraft.

Removed placard stating "GPS not to be used for navigation.

Weight and Balance change is negligible.

END

☐ Additional Sheets Are Attached



US Department  
of Transportation  
Federal Aviation  
Administration

**MAJOR REPAIR AND ALTERATION**  
**(Airframe, Powerplant, Propeller, or Appliance)**

Form Approved  
OMB No. 2120-0020

For FAA Use Only **6-11**  
Office Identification  
**GL-11 / IND FSDO**

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

1. Aircraft	Make <b>CESSNA</b>	Model <b>421C</b>
	Serial No. <b>421C-0897</b>	Nationality and Registration Mark <b>N 6174C</b>
2. Owner	Name (As shown on registration certificate) <b>TREADWAY AVIATION INC</b>	Address (As shown on registration certificate) <b>9406 PROMOTORY CIRCLE INDIANAPOLIS IN 46236</b>

**3. For FAA Use Only**

**4. Unit Identification**

Unit	Make	Model	Serial No.	5. Type	Repair	Alteration
AIRFRAME	~~~~~ (As described in Item 1 above) ~~~~~					X
POWERPLANT						
PROPELLER						
APPLIANCE	Type					
	Manufacturer					

**6. Conformity Statement**

A. Agency's Name and Address	B. Kind of Agency	C. Certificate No.
<b>Muncie Aviation Co. Box 1169, Muncie Airport Muncie, Indiana 47308</b>	<input type="checkbox"/> U.S. Certificated Mechanic	<b>AJVR135C</b>
	<input type="checkbox"/> Foreign Certificated Mechanic	
	<input checked="" type="checkbox"/> Certificated Repair Station	
	<input type="checkbox"/> Manufacturer	

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Date <b>2-14-96</b>	Signature of Authorized Individual <b>Kenneth K. Talhelm</b> <i>K. Talhelm</i>
------------------------	---

**7. Approval for Return To Service**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is ☒ APPROVED ☐ REJECTED

BY	FAA Flt. Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)
	FAA Designee <i>α</i>	Repair Station	Person Approved by Transport Canada Airworthiness Group	
Date of Approval or Rejection <b>2-14-96</b>		Certificate or Designation No. <b>AJVR135C</b>	Signature of Authorized Individual <b>Kenneth K. Talhelm</b> <i>K. Talhelm</i>	

## NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

### 8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N 6174C: Date 2-14-96

Removed Trimble TNL 2000T Satellite Navigation System.

The following equipment is installed in the instrument panel:

Installed this date Trimble TNL-2000 Approach GPS system TSO C129(A1) in the instrument panel, in accordance with STC SA09005SC. The CDI interface, and annunciator lights are installed as required for IFR non precision approach approval.

The altitude reporting system was interfaced with the GPS using a Transcal IF-RS232C-2 adaptor on the previous installation.


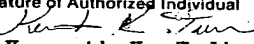
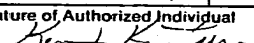
The altitude reporting system was fully checked in accordance with Appendix 1 of AC 43.6A per FAR 91.411 and 91.413.

The GPS antenna was installed in accordance with Advisory Circular 43.13-1A chapter 11, sections 2, 3, 5, and 7 and 43.13-2A, Chapter 3, paragraph 38 a and b; The GPS receiver was installed in accordance with Advisory Circular 20-138, paragraph 8c(2)(i), 8c(2)(ii). as a follow on to the above STC. A placard was installed stating "GPS is not used for navigation" pending FAA approval for IFR navigation.

Weight and balance and equipment list has been revised for this installation.

END

☐ Additional Sheets Are Attached

 <b>MAJOR REPAIR AND ALTERATION</b> (Airframe, Powerplant, Propeller, or Appliance)		Form Approved OMB No. 2120-0020 For FAA Use Only <i>CAK</i> Office Identification GL-11 / IND FSDO	
INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1953).			
1. Aircraft	Make <b>CESSNA</b>	Model <b>421C</b>	
	Serial No. <b>421C0897</b>	Nationality and Registration Mark <b>N 6174C</b>	
2. Owner	Name (As shown on registration certificate) <b>TREADWAY AVIATION INC</b>	Address (As shown on registration certificate) <b>9406 PROMONTORY CIR INDIANAPOLIS IN 46236</b>	
	3. For FAA Use Only		
4. Unit Identification			
Unit	Make	Model	Serial No.
AIRFRAME	(As described in Item 1 above)		
POWERPLANT			
PROPELLER			
APPLIANCE	Type		
	Manufacturer		
5. Type			
			Repair
			Alteration
			X
6. Conformity Statement			
A. Agency's Name and Address		B. Kind of Agency	
<b>Muncie Aviation Co.</b> <b>Box 1169, Muncie Airport</b> <b>Muncie, Indiana 47308</b>		U.S. Certificated Mechanic	
		Foreign Certificated Mechanic	
		X Certificated Repair Station	
		Manufacturer	
		C. Certificate No.	
		<b>AJVR135C</b>	
D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.			
Date <b>1-31-95</b>		Signature of Authorized Individual  <b>Kenneth K. Talhelm # 2406901</b>	
7. Approval for Return To Service			
Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is <input checked="" type="checkbox"/> APPROVED <input type="checkbox"/> REJECTED			
BY	FAA Fit. Standards Inspector	Manufacturer	Inspection Authorization
	FAA Designee	X Repair Station	Person Approved by Transport Canada Airworthiness Group
Date of Approval or Rejection <b>1-31-95</b>		Certificate or Designation No. <b>AJVR135C</b>	Signature of Authorized Individual  <b>Kenneth K. Talhelm Inspector AJVR135C</b>

**NOTICE**

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

**B. Description of Work Accomplished**

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Removed this date, Sperry AA-100 Radar altimeter system.


Installed this date, one King KRA 10A Radar Altimeter system.

All work was performed in accordance with Advisory Circular 43.13-2A, chapter 1, page 5 and chapter 2, paragraphs 21, 22, and 23 installed in spaces provided by the aircraft manufacturer.

Weight and Balance records have been revised.

END

☐ Additional Sheets Are Attached

 <b>MAJOR REPAIR AND ALTERATION</b> <b>(Airframe, Powerplant, Propeller, or Appliance)</b>				Form Approved OMB No. 2120-0020 <b>For FAA Use Only</b> Office Identification <b>GL-11 / IND FSDO</b>	
<b>INSTRUCTIONS:</b> Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).					
1. Aircraft	Make <b>CESSNA</b>		Model <b>421C</b>		
	Serial No. <b>421C0897</b>		Nationality and Registration Mark <b>N 6174C</b>		
2. Owner	Name (As shown on registration certificate) <b>TREADWAY AVIATION INC</b>		Address (As shown on registration certificate) <b>9406 PROMONTORY CIR INDIANAPOLIS IN 46236</b>		
	<b>3. For FAA Use Only</b>				
<b>4. Unit Identification</b>					
Unit	Make	Model	Serial No.	5. Type	
AIRFRAME	(As described in Item 1 above)			Repair	Alteration
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				
<b>6. Conformity Statement</b>					
A. Agency's Name and Address			B. Kind of Agency		C. Certificate No.
<b>Muncie Aviation Co.</b> <b>Box 1169, Muncie Airport</b> <b>Muncie, Indiana 47308</b>			<input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer		<b>AJVR135C</b>
D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.					
Date <b>1-13-95</b>			Signature of Authorized Individual <i>D. R. Hidy</i> <b>D. R. HIDY INSPECTOR AJVR135C</b>		
<b>7. Approval for Return To Service</b>					
Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is <input checked="" type="checkbox"/> <b>APPROVED</b> <input type="checkbox"/> <b>REJECTED</b>					
BY	FAA Fit Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)	
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canada Airworthiness Group		
Date of Approval or Rejection <b>1-13-95</b>		Certificate or Designation No. <b>AJVR135C</b>	Signature of Authorized Individual <i>D. R. Hidy</i> <b>D. R. HIDY INSPECTOR AJVR135C</b>		

DATE: 3- 6-95

**NOTICE**

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

**8. Description of Work Accomplished**

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Removed existing defective AeroSonic Fuel Flow system.

Installed this date Shadin Co. Inc. model 810532TD Fuel Flow Indicating system in accordance with Airframe STC # SA607GL type certificate # A7CE and Engine STC # SE612GL type certificate # E7CE. This system was installed per Shadin company report/installation manual # 4034 and 4043.

All work was performed in accordance with Advisory Circular 43.13-2A, chapter 11, Section 2, installed in spaces provided by the aircraft manufacturer.


The Shadin Digiflo-L operating manual was placed in the Aircraft Flight Manual.

Weight and Balance change is negligible.

END

☒ Additional Sheets Are Attached



 U.S. Department of Transportation Federal Aviation Administration		<b>MAJOR REPAIR AND ALTERATION</b> (Airframe, Powerplant, Propeller, or Appliance)		Form Approved OMB No. 2120-0020 For FAA Use Only Office Identification <b>GL-11 / IND FSDO</b>	
INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).					
1. Aircraft	Make <b>CESSNA</b>		Model <b>421C</b>		
	Serial No. <b>421C-0897</b>		Nationality and Registration Mark <b>N6174C</b>		
2. Owner	Name (As shown on registration certificate) <b>TREADWAY AVIATION, INC.</b>		Address (As shown on registration certificate) <b>9406 PROMONTORY CIRCLE INDIANAPOLIS, IN. 46236</b>		
3. For FAA Use Only					
4. Unit Identification					
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	(As described in Item 1 above)				*****
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				
5. Type					
6. Conformity Statement					
A. Agency's Name and Address		B. Kind of Agency		C. Certificate No.	
<b>MICHAEL EDWARD MCMURRER P.O. BOX 560235AY MONTVERDE, FL. 34756</b>		<input checked="" type="checkbox"/> U.S. Certified Mechanic		<b>264455865AP</b>	
		<input type="checkbox"/> Foreign Certified Mechanic			
		<input type="checkbox"/> Certified Repair Station			
		<input type="checkbox"/> Manufacturer			
D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.					
Date		Signature of Authorized Individual			
28 NOV 94		<b>MIKE MCMURRER</b> <i>Mike McMurver</i>			
7. Approval for Return To Service					
Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is <input checked="" type="checkbox"/> APPROVED <input type="checkbox"/> REJECTED					
BY	FAA Flt. Standards Inspector	Manufacturer <input checked="" type="checkbox"/>	Inspection Authorization	Other (Specify)	
	FAA Designee	Repair Station	Person Approved by Transport Canada Airworthiness Group		
Date of Approval or Rejection		Certificate or Designation No.	Signature of Authorized Individual		
28 NOV 94		310648127IA	<b>ROBERT M. WILLIS</b> <i>Robert M. Willis</i>		

**NOTICE**

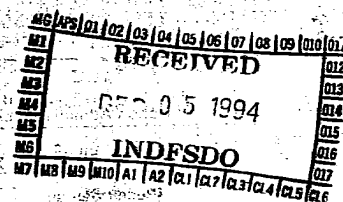
Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

**8. Description of Work Accomplished**


(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

INSTALLED VORTEX GENERATOR SYSTEMS ON THE WING AND VERTICAL STEBILIZER, IN ACCORDANCE WITH STC SA00015SE AND THE INSTALLATION INSTRUCTIONS MANUAL IM92-6 AND FAA APPROVED COPY OF AERONAUTICAL TESTING SERVICE DRAWING LIST NO. ATS-421C. BOTH DATED MAY 2, 1994. THIS MODIFICATION INCLUDES REDUCED Vmca AND STALL SPEEDS AND A GROSS WEIGHT INCREASE. REVISED FLIGHT MANUAL WITH COPY OF FAA APPROVED AIRPLANE FLIGHT MANUAL SUPPLEMENT REPORT NO. ATS-93-02a, DATED AUGUST 26, 1993. WEIGHT CHANGE NEGLIGIBLE.

-----  
END



☐ Additional Sheets Are Attached

 <b>MAJOR REPAIR AND ALTERATION</b> <b>(Airframe, Powerplant, Propeller, or Appliance)</b>		Form Approved OMB No. 2120-0020 For FAA Use Only <i>Call</i> Office Identification <b>GL-11 / IND FSDO</b>	
INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).			
1. Aircraft	Make <b>CESSNA</b>	Model <b>421C</b>	
	Serial No. <b>421C-0897</b>	Nationality and Registration Mark <b>N 6174C</b>	
2. Owner	Name (As shown on registration certificate) <b>TREADWAY AVIATION INC</b>	Address (As shown on registration certificate) <b>9406 PROMOTORY CIRCLE INDIANAPOLIS IN 46236</b>	
3. For FAA Use Only  "The data identified herein complies with the applicable airworthiness requirements and is approved for the above described aircraft, subject to conformity inspection by a person authorized in FAR 43, section 43.7. Date: <b>NOV 22 1994</b> FAA Inspector AGL-IND-FSDO <i>Colin J. Henders</i>			
4. Unit Identification			5. Type
Unit	Make	Model	Serial No.
AIRFRAME	(As described in Item 1 above)		
POWERPLANT			
PROPELLER			
APPLIANCE	Type		
	Manufacturer		
6. Conformity Statement A. Agency's Name and Address <b>Muncie Aviation Co. Box 1168, Muncie Airport Muncie, Indiana 47308</b> B. Kind of Agency <input type="checkbox"/> U.S. Certified Mechanic <input type="checkbox"/> Foreign Certified Mechanic <input checked="" type="checkbox"/> Certified Repair Station <input type="checkbox"/> Manufacturer C. Certificate No. <b>AJVR135C</b> D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.			
Date <b>11-11-94</b>		Signature of Authorized Individual <i>Kenneth K. Talhelm</i> <b>Kenneth K. Talhelm # 2406801</b>	
7. Approval for Return To Service Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is <input checked="" type="checkbox"/> APPROVED <input type="checkbox"/> REJECTED			
BY	FAA FTL Standards Inspector	Manufacturer	Inspection Authorization
	FAA Designee	<input checked="" type="checkbox"/> Repair Station	Person Approved by Transport Canada Airworthiness Group
Date of Approval or Rejection <b>11-23-94</b>		Certificate or Designation No. <b>AJVR135C</b>	Signature of Authorized Individual <i>Kenneth K. Talhelm</i> <b>Kenneth K. Talhelm Inspector AJVR135C</b>

**NOTICE**

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

**8. Description of Work Accomplished**

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Certified previously installed Trimble TNL 2000T GPS system for IFR flight.

This units operational revision status is NAV 2.250, GPS 1.12

The aircraft was test flown and found to meet the requirements for IFR enroute and terminal navigation per AC 20-121a, paragraph 9.

Test Pilot signature certifying IFR accuracy requirements.

*Kenneth K. Talhelm*  
KENNETH K. TALHELM

#2406901

Date: 11-11-94

The aircraft has been placarded "GPS is not approved for approaches".



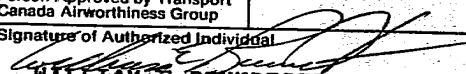
A revised TNL 2000T Pilots guide, P/N 80817 No rev. dated August 19, 1993 or later version, has been placed in the aircraft.

The FAA Approved Flight Manual Supplement accompanying this Field Approval is issued to this aircraft.

Weight and Balance change is negligible.

END

☐ Additional Sheets Are Attached

 <b>MAJOR REPAIR AND ALTERATION</b> <b>(Airframe, Powerplant, Propeller, or Appliance)</b>				Form Approved OMB No. 2120-0020 For FAA Use Only <i>CLW</i> Office Identification <b>GL-11 / IND FSDO</b>	
<b>INSTRUCTIONS.</b> Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).					
<b>1. Aircraft</b>	Make	CESSNA		Model	421C
	Serial No.	421C-0897		Nationality and Registration Mark	N 6174C
<b>2. Owner</b>	Name (As shown on registration certificate) TREADWAY AVIATION INC			Address (As shown on registration certificate) 8406 PROMOTORY CIRCLE INDIANAPOLIS IN 46236	
<b>3. For FAA Use Only</b>					
<b>4. Unit Identification</b>					
Unit	Make	Model	Serial No.	Type	
AIRFRAME	(As described in item 1 above)			Repair	Alteration
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				
<b>6. Conformity Statement</b>					
A. Agency's Name and Address		B. Kind of Agency		C. Certificate No.	
Muncie Aviation Co. Box 1189, Muncie Airport Muncie, Indiana 47308		<input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer		AJVR135C	
D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.					
Date		Signature of Authorized Individual			
11-11-84		 WILLIAM E ROUNDTREE			
<b>7. Approval for Return To Service</b>					
Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is <input checked="" type="checkbox"/> APPROVED <input type="checkbox"/> REJECTED					
BY	FAA Fit Standards Inspector	Manufacturer	Inspection Authorization	Other (Specify)	
	FAA Designee	Repair Station	Person Approved by Transport Canada Airworthiness Group		
Date of Approval or Rejection		Certificate or Designation No.	Signature of Authorized Individual		
11-11-84		AJVR135C	 WILLIAM E ROUNDTREE		
			AJVR135C		

**NOTICE**

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

**B. Description of Work Accomplished**


(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Removed this date, one Tomorrow Appolo 612 Loran C system.  
Removed this date one cessna electric clock.  
Installed 1 LC-8 digital electric clock.  
Installed this date Trimble TNL 2000T (TSO-C129) Satellite Navigation System. The GPS antenna is TSO-C129 status.  
Installed Annunciators and CDI interconnections IFR approval.  
The GPS CDI output was connected through a failsafe switching system to the pilot HSI with ILS priority and controlled by an annuciated switch. Installed Transcal Industries altitude encoder to RS232C interface adapter model # IA-232C-2, to the existing encoder and the trimble GPS receiver.  
The GPS antenna was installed in accordance with Advisory Circular 43.13-2A, Chapter 3, paragraph 38 a and b; The GPS receiver was installed in accordance with Advisory Circular 20-121A Chapter 7 paragraph a 1 thru 5 in spaces provided by the manufacturer and A placard was installed stating "GPS NOT APPROVED FOR IFR."  
All work was performed using manufacturers schematics, drawings, and in accordance with Advisory Circular 43.13-2A, chapter 1, page 5 and chapter 2, paragraphs 21, 22, and 23 installed in spaces provided by the aircraft manufacturer.  
The altitude reporting system was fully checked in accordance with Appendix 1 of AC 43.6A per FAR 91.411 and 91.413.  
All systems were checked per manufacturers installation and testing procedures.  
Weight and Balance records have been revised.

END

☐ Additional Sheets Are Attached.



 US Department of Transportation Federal Aviation Administration		<b>MAJOR REPAIR AND ALTERATION</b> (Airframe, Powerplant, Propeller, or Appliance)		Form Approved OMB No. 2120-0020 For FAA Use Only Office Identification <i>ASC FSPD 03</i>	
INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).					
1. Aircraft		Make <b>Cessna</b> Serial No. <b>421C0897</b>		Model <b>421C</b> Nationality and Registration Mark <b>N6174C</b>	
2. Owner		Name (As shown on registration certificate) <b>World Tower Company</b>		Address (As shown on registration certificate) <b>P.O. Box 405 Mayfield, KY. 42066</b>	
3. For FAA Use Only					
4. Unit Identification					
Unit	Make	Model	Serial No.	Repair	Alteration
AIRFRAME	(As described in Item 1 above)				X
POWERPLANT					
PROPELLER					
APPLIANCE	Type				
	Manufacturer				
6. Conformity Statement					
A. Agency's Name and Address		B. Kind of Agency		C. Certificate No.	
<b>Smyrna Air Center Hangar 621 Smyrna Airport Smyrna, TN. 37167</b>		<input type="checkbox"/> U.S. Certificated Mechanic <input type="checkbox"/> Foreign Certificated Mechanic <input checked="" type="checkbox"/> Certificated Repair Station <input type="checkbox"/> Manufacturer		<b>RC4R49 IM Radio Limited Airframe</b>	
D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished hereon is true and correct to the best of my knowledge.					
Date <b>2-26-93</b>		Signature of Authorized Individual <b>Ron Dupler</b> <i>Ron Dupler</i>			
7. Approval for Return To Service					
Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is <input checked="" type="checkbox"/> APPROVED <input type="checkbox"/> REJECTED					
BY	Inspector	Manufacturer	Inspection Authorization	Other (Specify)	
	FAA Designee	X Repair Station	Person Approved by Transport Canada Airworthiness Group		
Date of Approval or Rejection <b>2-26-93</b>		Certificate or Designation No. <b>RC4R49 IM</b>	Signature of Authorized Individual <b>Ron Dupler</b> <i>Ron Dupler</i>		

**NOTICE**

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

**8. Description of Work Accomplished**

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

INSTALLED NEW NARCO AR850 BLIND ENCODER IN SPARE INSTRUMENT HOLE IN PILOT'S INSTRUMENT PANEL. WIRED ENCODER TO EXISTING ARC 400 SERIES TRANSPONDER.  
THE ABOVE INSTALLATION WAS DONE AS PER MANUFACTURERS INSTALLATION MANUAL AND A.C.43.13-1A, CHAPTER 11, SECTIONS 1 AND 2, CHAPTER 15, SECTIONS 1 AND 2; A.C.43.13-2A, CHAPTERS 1 THRU 3.  
RECERTIFIED STATIC SYSTEM AND RAN ENCODER CORRELATION TESTS PER FAR 91.411. ALL WORK ON FILE AT SMYRNA AIR CENTER ON WORK ORDER #14251.

	WEIGHT	ARM	MOMENT
OLD WEIGHT & BALANCE:	5443.39	152.03	827568.9
AR850 ENCODER	.80	111.50	89.2

NEW EMPTY WEIGHT: 5444.19  
NEW CENTER OF GRAVITY: 152.03  
NEW MOMENT: 827658.1

THE NEW CENTER OF GRAVITY FALLS WITHIN THE LIMITS AS SPECIFIED IN THE FLIGHT MANUAL. THE NEW ELECTRICAL LOAD DOES NOT EXCEED 80% OF THE AIRCRAFT CAPABILITY. IT IS THE RESPONSIBILITY OF THE PILOT TO INSURE CORRECT WEIGHT AND BALANCE BEFORE EACH FLIGHT.

-----END-----

RECEIVED  
FAA

MAR 2 1993

TN FSDO  
Nashville, Tennessee

☐ Additional Sheets Are Attached



U.S. DEPARTMENT OF TRANSPORTATION FEDERAL AVIATION ADMINISTRATION				Form Approved Budget Bureau No. 04-R060.1	
<b>MAJOR REPAIR AND ALTERATION</b> (Airframe, Powerplant, Propeller, or Appliance)				FOR FAA USE ONLY	
				OFFICE IDENTIFICATION AGL-GADO-10	
INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form.					
1. AIRCRAFT	MAKE <u>Cessna</u>	MODEL <u>421C</u>		NATIONALITY AND REGISTRATION MARK <u>N6174C</u>	
	SERIAL NO. <u>42100897</u>				
2. OWNER	NAME (As shown on registration certificate) <u>WORLD TOWER Company</u>		ADDRESS (As shown on registration certificate) <u>P O Box 405 Mayfield, Ky 42066</u>		
	<p style="text-align: center;">3. FOR FAA USE ONLY</p> <p style="text-align: center;">The data identified herein complies with the applicable airworthiness requirements and is approved for the above described aircraft, subject to conformity inspection by a person authorized in FAR 43, section 43.7 "</p> <p style="text-align: center;"><u>9-30-86</u> <u>Chas. D. Headley</u> Date FAA Inspector <u>AGL-GADO-10</u></p>				
4. UNIT IDENTIFICATION					
UNIT	MAKE	MODEL	SERIAL NO.	5. TYPE	
				REPAIR	ALTERATION
AIRFRAME	***** (As described in item 1 above) *****				X
POWERPLANT					
PROPELLER					
APPLIANCE	TYPE				
	MANUFACTURER				
6. CONFORMITY STATEMENT					
A. AGENCY'S NAME AND ADDRESS			B. KIND OF AGENCY		C. CERTIFICATE NO.
<b>Ron Collins Aviation Electronics</b> <b>6103 Flightline Dr</b> <b>Evansville, Indiana 47711</b>			U.S. CERTIFICATED MECHANIC		<b>3283</b>
			FOREIGN CERTIFICATED MECHANIC		
			<input checked="" type="checkbox"/> CERTIFICATED REPAIR STATION		
			<input type="checkbox"/> MANUFACTURER		
D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.					
DATE <b>9-30-86</b>			SIGNATURE OF AUTHORIZED INDIVIDUAL <u>Gordon L. Lerry</u>		
7. APPROVAL FOR RETURN TO SERVICE					
Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is <input checked="" type="checkbox"/> APPROVED <input type="checkbox"/> REJECTED					
BY	FAA FLT. STANDARDS INSPECTOR	MANUFACTURER	INSPECTION AUTHORIZATION		OTHER (Specify)
	FAA DESIGNEE <input checked="" type="checkbox"/>	REPAIR STATION	CANADIAN DEPARTMENT OF TRANSPORT INSPECTOR OF AIRCRAFT		
DATE OF APPROVAL OR REJECTION <b>9-30-86</b>		CERTIFICATE OR DESIGNATION NO. <b>3283</b>	SIGNATURE OF AUTHORIZED INDIVIDUAL <u>Gordon L. Lerry</u>		

# NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. DESCRIPTION OF WORK ACCOMPLISHED (If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Removed ARC R1048A Nav receiver, RTA876A DME transceiver, C876A DME control, 800 RNAV and RN868A computer.

Installed II Morrow 612 Loran C IFR receiver, A23 Loran C antenna, King KN 64 DME and ARC R1048B Nav receiver, FC-02 Flag adapter.

A switch, relays and a blue annunciator light are provided for Loran sharing of CDI with the #2 Nav system. The CDI is automatically switched to VOR/LOC mode when the #2 Nav radio is switched to ILS frequency.

All work performed in accordance with AC43.13-1A-2A and in accordance with manufacturers installation instructions.

A test flight was performed to certify compliance with AC 90-45A for VFR, IFR Enroute operation by Edmund Brand Lic.No. 1322365 on Oct. 4, 1986.

Aircraft has been placarded "Loran C not certified for Approaches." An FAA approved Flight Manual supplement and Apollo II Model 612 IFR operation manual have been included in the aircraft records.

Log book entry reference this form 337. Aircraft weight and balance report and equipment list revised.

END

M1		M2		M3		M4		M5	
CL1		CL2		CL3		CL4		AV	

RECEIVED  
OCT 16 1986  
GADO-10

☐ ADDITIONAL SHEETS ARE ATTACHED

U.S. DEPARTMENT OF TRANSPORTATION FEDERAL AVIATION ADMINISTRATION <b>MAJOR REPAIR AND ALTERATION</b> (Airframe, Powerplant, Propeller, or Appliance)				Form Approved Budget Bureau No. 04-R060.1 <b>FOR FAA USE ONLY</b> OFFICE IDENTIFICATION AGL-GADO-10	
INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form.					
1. AIRCRAFT	MAKE <b>Cessna</b>		MODEL <b>421C</b>		
	SERIAL NO. <b>421C0897</b>		NATIONALITY AND REGISTRATION MARK <b>N6174C</b>		
2. OWNER	NAME (As shown on registration certificate) <b>WORLD TOWER COMPANY</b>		ADDRESS (As shown on registration certificate) <b>P O Box 405 Mayfield, Ky 42066</b>		
	3. FOR FAA USE ONLY				
4. UNIT IDENTIFICATION					5. TYPE
UNIT	MAKE	MODEL	SERIAL NO.	REPAIR	ALTERATION
AIRFRAME	***** (As described in item 1 above) *****				X
POWERPLANT					
PROPELLER					
APPLIANCE	TYPE				
	MANUFACTURER				
6. CONFORMITY STATEMENT					
A. AGENCY'S NAME AND ADDRESS			B. KIND OF AGENCY		C. CERTIFICATE NO.
<b>Ron Collins Aviation Electronics</b> <b>6103 Flightline Dr</b> <b>Evansville, Indiana 47711</b>			U.S. CERTIFICATED MECHANIC		<b>3283</b>
			FOREIGN CERTIFICATED MECHANIC		
			<input checked="" type="checkbox"/> CERTIFICATED REPAIR STATION		
			MANUFACTURER		
D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.					
DATE <b>Sept 30, 1986</b>			SIGNATURE OF AUTHORIZED INDIVIDUAL <i>Gordon L. Terry</i>		
7. APPROVAL FOR RETURN TO SERVICE					
Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is <input checked="" type="checkbox"/> APPROVED <input type="checkbox"/> REJECTED					
BY	FAA FLT. STANDARDS INSPECTOR	MANUFACTURER	INSPECTION AUTHORIZATION		OTHER (Specify)
	FAA DESIGNEE	<input checked="" type="checkbox"/> REPAIR STATION	CANADIAN DEPARTMENT OF TRANSPORT INSPECTOR OF AIRCRAFT		
DATE OF APPROVAL OR REJECTION <b>9-30-86</b>		CERTIFICATE OR DESIGNATION NO. <b>3283</b>		SIGNATURE OF AUTHORIZED INDIVIDUAL <i>Gordon L. Terry</i>	

# NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. DESCRIPTION OF WORK ACCOMPLISHED (If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Removed ARC R1048A Nav Receiver, RTA876A DME transceiver, C876A DME control, 800 RNAV and RN878A computer.

Installed II Morrow 612 Loran C IFR receiver, A23 Loran C antenna King KN64 DME and ARC R1048B Nav receiver.

A switch, relays and a blue annunciator light are provided for Loran sharing of CDI with the #2 Nav system. The CDI is automatically switched to VOR/LOC Mode when the #2 Nav radio is switched to the ILS frequency.

All work performed in accordance with AC 43.13-1A-2A and in accordance with manufacturers installation instructions.

Aircraft has been placarded "Loran C not certified for IFR flight." Pending FAA approval of flight manual supplement and IFR certification.

Log book entry references this form 337. Aircraft weight and balance report and equipment list revised.

END

MG	APS	01	02	03	04
M1					05
M2					06
M3					07
M4					08
M5	CL1	CL2	CL3	CL4	AV

RECEIVED  
OCT 6 1986  
GADO-10

☐ ADDITIONAL SHEETS ARE ATTACHED

5435

DEPARTMENT OF TRANSPORTATION FEDERAL AVIATION ADMINISTRATION <b>MAJOR REPAIR AND ALTERATION</b> (Airframe, Powerplant, Propeller, or Appliance)				Form Approved Budget Bureau No. 04-R060.1  FOR FAA USE ONLY OFFICE IDENTIFICATION AGL-GADO-10	
INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form.					
1. AIRCRAFT	MAKE <b>Cessna</b>		MODEL <b>421C</b>		
	SERIAL NO. <b>421C0897</b>		NATIONALITY AND REGISTRATION MARK <b>N6174C</b>		
2. OWNER	NAME (As shown on registration certificate) <b>World Tower Inc</b>		ADDRESS (As shown on registration certificate) <b>P O Box 405 Mayfield, Kentucky 42066</b>		
3. FOR FAA USE ONLY					
4. UNIT IDENTIFICATION					5. TYPE
UNIT	MAKE	MODEL	SERIAL NO.	REPAIR	ALTERATION
AIRFRAME	(As described in item 1 above) *****				X
POWERPLANT					
PROPELLER					
APPLIANCE	TYPE				
	MANUFACTURER				
6. CONFORMITY STATEMENT					
A. AGENCY'S NAME AND ADDRESS			B. KIND OF AGENCY		C. CERTIFICATE NO.
<b>RON COLLINS AVIATION ELECTRONICS</b> <b>6103 Flightline Dr</b> <b>Evansville, Indiana 47711</b>			U.S. CERTIFICATED MECHANIC		3283
			FOREIGN CERTIFICATED MECHANIC		
			X CERTIFICATED REPAIR STATION		
			MANUFACTURER		
D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.					
DATE <b>March 26, 1986</b>			SIGNATURE OF AUTHORIZED INDIVIDUAL <i>Gordon L. Terry</i>		
7. APPROVAL FOR RETURN TO SERVICE					
Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is <input checked="" type="checkbox"/> APPROVED <input type="checkbox"/> REJECTED					
BY	FAA FLT. STANDARDS INSPECTOR	MANUFACTURER	INSPECTION AUTHORIZATION		OTHER (Specify)
	FAA DESIGNEE	X REPAIR STATION	CANADIAN DEPARTMENT OF TRANSPORT INSPECTOR OF AIRCRAFT		
DATE OF APPROVAL OR REJECTION <b>3-26-86</b>		CERTIFICATE OR DESIGNATION NO. <b>3283</b>	SIGNATURE OF AUTHORIZED INDIVIDUAL <i>Gordon L. Terry</i>		

# NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. DESCRIPTION OF WORK ACCOMPLISHED (If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Installed Wulfsburg Flitefone VI Radiotelephone system consisting of RT-18D transceiver, WH-10 handset, AT-461 antenna.  
All work performed in accordance with AC43.13-1A, -2A and manufacturers installation instructions.  
Aircraft Weight and Balance and Equipment list revised.

REG	APS	01	02	03	04
M1	RECEIVED				05
M2	APR 3 1986				06
M3	GADO -10				07
M4					08
M5	CL1	CL2	CL3	CL4	AV

☐ ADDITIONAL SHEETS ARE ATTACHED

81-2582

Form Approved  
Budget Bureau No. 04-R0058

DEPARTMENT OF TRANSPORTATION FEDERAL AVIATION ADMINISTRATION APPLICATION FOR AIRWORTHINESS CERTIFICATE				INSTRUCTIONS—Print or type. Do not write in shaded areas; these are for FAA use only. Submit original only to an authorized FAA Representative. If additional space is required, use an attachment. For special flight permits complete Sections II and VI or VII as applicable.			
I. AIRCRAFT DESCRIPTION	1. REGISTRATION MARK	2. AIRCRAFT BUILDER'S NAME (make)	3. AIRCRAFT MODEL DESIGNATION	4. YR. MFG.	FAA CODING		
	N 6174C	Cessna	421C		2076017		
	5. AIRCRAFT SERIAL NO.	6. ENGINE BUILDER'S NAME (make)	7. ENGINE MODEL DESIGNATION				
	421C0897	Continental	GTSIO-520-L	17039			
II. CERTIFICATION REQUESTED	8. NUMBER OF ENGINES	9. PROPELLER BUILDER'S NAME (make)	10. PROPELLER MODEL DESIGNATION	11. AIRCRAFT IS			
	Two	McCauley	3FF32C501/90UMB-0	IMPORT			
APPLICATION IS HEREBY MADE FOR: (Check applicable items)							
A. 1 X STANDARD AIRWORTHINESS CERT. (Indicate category) X NORMAL UTILITY ACROBATIC TRANSPORT GLIDER BALLOON							
B. SPECIAL AIRWORTHINESS CERTIFICATE (Check appropriate items)							
2 LIMITED							
5 PROVISIONAL (Indicate class)							
3 RESTRICTED (Indicate operation(s) to be conducted)							
4 EXPERIMENTAL (Indicate operation(s) to be conducted)							
8 SPECIAL FLIGHT PERMIT (Indicate operation to be conducted: A even complete Section VI or VII as applicable on reverse side)							
C. 6 MULTIPLE AIRWORTHINESS CERTIFICATE (Check appropriate Restricted Operation and Standard or Limited as applicable above)							
III. OWNER'S CERTIFICATION	A. REGISTERED OWNER (As shown on Certificate of Aircraft Registration)				IF DEALER, CHECK HERE X		
	NAME Cessna Aircraft Company *				ADDRESS West K-42 Highway, P.O. Box 7704 Wichita, Kansas 67277		
	B. AIRCRAFT CERTIFICATION BASIS (Check applicable blocks and complete items as indicated)						
	AIRCRAFT SPECIFICATION OR TYPE CERTIFICATION DATA SHEET (Give No. and Revision No.) A7CE Rev. 19				AIRWORTHINESS DIRECTIVES (Check if all applicable AD's complied with and give latest ID No.) Issue: 80-15-13		
AIRCRAFT LISTING (Give page No.) N/A				SUPPLEMENTAL TYPE CERTIFICATE (List number of each STC incorporated) SA3003WE			
C. AIRCRAFT OPERATION AND MAINTENANCE RECORDS							
CHECK IF RECORDS IN COMPLIANCE WITH FAR 91.173 X				TOTAL AIRFRAME HOURS—XXXXXX		3 EXPERIMENTAL ONLY—Enter hours flown since last certificate issued or renewal N/A	
D. CERTIFICATION—I hereby certify that I am the owner (or his agent) of the aircraft described above; that the aircraft is registered with the Federal Aviation Administration in accordance with Section 501 of the Federal Aviation Act of 1958, and applicable Federal Aviation Regulations; and that the aircraft has been inspected and is airworthy and eligible for the airworthiness certificate requested.							
DATE OF APPLICATION 8-25-80				NAME AND TITLE (Print or type) M. Gann, Quality Control Manager		SIGNATURE	
IV. INSPECTION AGENCY VERIFICATION	A. THE AIRCRAFT DESCRIBED ABOVE HAS BEEN INSPECTED AND FOUND AIRWORTHY BY: (Complete this section only if FAR 21.163 (d) applies)						
	2 FAR PART 121 OR 127 CERTIFICATE HOLDER (Give Certificate No.)	3 CERTIFIED MECHANIC (Give Certificate No.)	6 CERTIFIED REPAIR STATION (Give Certificate No.)				
V. FAA REPRESENTATIVE CERTIFICATION	5 AIRCRAFT MANUFACTURER (Give Name of Firm)						
	DATE 8-25-80	TITLE CE EMDO 3-0-43	DESIGNATION OF SPECIAL AGENT IN CHARGE Raymond M. Rowden			FAA INSPECTOR'S SIGNATURE	

<b>VI. PRODUCTION FLIGHT TESTING</b>	<b>A. MANUFACTURER</b>	
	NAME	ADDRESS
	<b>B. PRODUCTION BASIS (Check applicable item)</b>	
	<input type="checkbox"/> PRODUCTION CERTIFICATE (Give production certificate number) <input type="checkbox"/> TYPE CERTIFICATE ONLY <input type="checkbox"/> APPROVED PRODUCTION INSPECTION SYSTEM	
<b>VII. SPECIAL FLIGHT PERMIT PURPOSES OTHER THAN PRODUCTION FLIGHT TEST</b>	<b>C. GIVE QUANTITY OF CERTIFICATES REQUIRED FOR OPERATING NEEDS:</b>	
	DATE OF APPLICATION	SIGNATURE
	NAME AND TITLE (Print or type)	
	<b>A. DESCRIPTION OF AIRCRAFT</b>	
	REGISTERED OWNER	ADDRESS
	BUILDER (Make)	MODEL
	SERIAL NUMBER	REGISTRATION MARK
	<b>B. DESCRIPTION OF FLIGHT</b>	
	FROM	TO
	VIA	DEPARTURE DATE
DURATION		
<b>VIII. AIRWORTHINESS DOCUMENTATION (FAA use only)</b>	<b>C. CREW REQUIRED TO OPERATE THE AIRCRAFT AND ITS EQUIPMENT</b>	
	<input type="checkbox"/> PILOT <input type="checkbox"/> CO-PILOT <input type="checkbox"/> NAVIGATOR <input type="checkbox"/> OTHER (Specify)	
	<b>D. THE AIRCRAFT DOES NOT MEET THE APPLICABLE AIRWORTHINESS REQUIREMENTS AS FOLLOWS:</b>	
	<b>E. THE FOLLOWING RESTRICTIONS ARE CONSIDERED NECESSARY FOR SAFE OPERATION (Use attachment if necessary)</b>	
	<b>F. CERTIFICATION</b> —I hereby certify that I am the registered owner (or his agent) of the aircraft described above; that the aircraft is registered with the Federal Aviation Administration in accordance with Section 501 of the Federal Aviation Act of 1958, and applicable Federal Aviation Regulations; and that the aircraft has been inspected and is airworthy for the flight described.	
	DATE	SIGNATURE
	NAME AND TITLE (Print or type)	
<b>VIII. AIRWORTHINESS DOCUMENTATION (FAA use only)</b>	<input checked="" type="checkbox"/> A. Operating Limitations and Markings in Compliance with FAR 91.31 as Applicable	
	<input type="checkbox"/> B. Current Operating Limitations Attached	
	<input type="checkbox"/> C. Data, Drawings, Photographs, etc. (Attach when required)	
	<input checked="" type="checkbox"/> D. Current Weight and Balance Information Available in Aircraft	
	<input type="checkbox"/> E. Major Repair and Alteration, FAA 337 (Attach when required)	
	<input checked="" type="checkbox"/> F. This Inspection Recorded in Aircraft Records	
<input type="checkbox"/> G. Statement of Conformity, FAA Form 317 (Attach when required)		
<input type="checkbox"/> H. Foreign Airworthiness Certification for Import Aircraft (Attach when required)		
<input type="checkbox"/> I. Previous Airworthiness Certificate Issued in Accordance with FAR _____ CAR _____ (Original attached)		
<input type="checkbox"/> J. Current Airworthiness Certificate Issued in Accordance with FAR <u>21.183a per 21.273</u> (Copy attached)		