N617CC 1980 Cessna 421C

FAA Form 337's

Aircraft S/N: 421C-0897



Prepared by the worldwide aviation specialists at RidgeAire, Inc.

U.S. Department of Transportation
Federal Aviation Administration

MAJOR REPAIR AND ALTERATION

Form Approved OMB No.2120-0020 2/28/2011	Electronic Tracking Number
For F	AA Use Only

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1. Ai	rcraft	Nationality and Registration Mark 617CC							Serial No. 421C-0897			
		Make Cessna	a				Model 421C					Series 400
2. 0	Name (As shown on registration certificate) Vivace International Corp								ificate) 0 State TX JSA			
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			POWER	PLAN	IT .							
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A. Ag	gency's	Name and	d Addres	S		.	Τ	B. Kind of				
Name	Air I	mpressions,	Inc.					U.S. Certifica	ted Mechanic			Manufacturer
Address	7929	Karl May D	r.					Foreign Cert	ficated Mechanic		C. (Certificate No.
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FAA Form 337 (10-06)

NOTICE

	N617CC		03/20/2015
	Nationality and Registration Mark	K	Date
talled fiberglass dome hub caps ed 10/29/2014 per STC SA0243	I/AW FAA Approved Premiere-Aviations I 0LA.	Master Document List No	o. PAS400HC-MDL Rev. "B"
talled external AFT body strakes 01935LA per STC SA01935LA a	I/A/W FAA Approved Aircraft Performance and 8110-3 dated 11/7/2013.	e Modifications Master D	owg. List as listed on AML No
w weight and balance computed	·		
tinent details are on file under w	ork order #3963.		
	END		
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1. DATE U.S. DEPARTMENT OF TRANSPORTATION FEDERAL AVIATION ADMINISTRATION November 7, 2013 STATEMENT OF COMPLIANCE WITH AIRWORTHINESS STANDARDS AIRCRAFT OR AIRCRAFT COMPONENT IDENTIFICATION 3. MODEL NO. 5. NAME OF APPLICANT 2. MAKE 4. TYPE (Aircraft, Engine, Propeller, etc.) *See Below Airplane Aircraft Performance Modification Cessna Colorado Springs, CO. LIST OF DATA 6. IDENTIFICATION 7. TITLE * Models: 335, 340, 340A, 401, 401A, 401B, 402, 402A, 402B, 402C, 411, 411A, 414, 414A, 421, 421A, 421B, 421C, 425, 441. **Engineering Change** Order: EO 1 G3-050504-001 Cessna 400 Series Aft Body Strakes Rev K dated 04/15/2013 dated 11/01/2013 for G3-050504-001 Rev K 1) Approval limited to Structural aspects (Statics and Fatigue) only. Structures Note: 2) The CR3212-4 are structurally satisfactory replacements for NAS1097AD4 rivets. 3) The Report No. HAAS-050504-001SA Rev (G) Stress Analysis remains structurally satisfactory without change. 8. PURPOSE OF DATA To provide type data for FAA approval of structure in support of updates to fasteners in the aft strake installation. Ref FAA STC No. SA01935LA 9. APPLICABLE REQUIREMENTS (List specific sections) Ref. TCDS 3A25, A28CE, A7CE 14CFR Part 23 through amendment 23-21, Paragraphs: 23.601 Amdt 23-0, 23.603 Amdt 23-0, 23.605 Amdt 23-0, 23.609 Amdt 23-0, 23.611 Amdt 23-7, 23.613 Amdt 23-0, 23.615(a) Amdt 23-7, 23.625 Amdt 23-7, 23.627 Amdt 23-0. 10. CERTIFICATION - Under authority vested by direction of the Administrator and in accordance with conditions and limitations of appointment under 14 CFR Part 183, data listed above and on attached sheets numbered <u>n/a</u> have been examined in accordance with established procedures and found to comply with applicable requirements of the Airworthiness Standards listed. Recommend approval of these data Approve these data I (VMS) Therefore 11. SIGNATURE(S) OF DESIGNATED ENGINEERING REPRESENTATIVE(S) 12. DESIGNATION NUMBERS(S) 13. CLASSIFICATION(S) DERT-230307-CE Donald E. Shepherd Jr. Structures Donald []

ENGINEERING CHANGE ORDER

ML - 1001 11-74

SHEET 1 OF 1

DWG. TITLE G3-050504-	-001 Cessi	na 400	Series	Aft	DWG. NO.	MOD. NO.	EO	
Body Strake	s Rev K d	ated 0	G3-050504-001		EO 1			
Jerry Lowe	DESIGN	CHECK	MECH.	ELECT.	STRESS D. Sheph	GROUP LEAD	DATE	Nov. 1, 2013
REASON FOR CHANGE								

G3-050504-001 Rivet Substitution

MODIFICATION TO G3-050504-001 Cessna 400 Series Aft Body Strake Note

1) Note 7: ADD Permissible to substitute NAS1097AD4-3 rivets with CR3212-4 using proper countersink.

PLOT TIME:

ALL OTHER INFORMATION REMAINS UNCHANGED. O. Shiptief



US Department
of Transportation
Federal Aviation
Administration

MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved OMB No. 2120-0020 2/28/2011	Electronic Tracking Number
F	or FAA Use Only

INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a)) Nationality and Registration Mark Serial No. IUSA N617CC 421C0897 1. Aircraft Make Model Series 421C Cessna Address (As shown on registration certificate) Name (As shown on registration certificate) Address 3103 9yh Ave Dr NW 2. Owner **Hickory** State NC 28601 Aerologistics I, LLC Country USA 3. For FAA Use Only The technical data identified herein has been found to comply applicable airworthiness requirements and is hereby approved for use only on the above described aircraft, subject to conformly inspection berson authorized in 643.7 5. Unit Identification 4. Type Repair Alteration Unit Make Model Serial No. (As described in Item 1 above) \square AIRFRAME **POWERPLANT PROPELLER** Туре APPLIANCE Manufacturer 6. Conformity Statement A. Agency's Name and Address B. Kind of Agency U. S. Certificated Mechanic Manufacturer Riverhawk Aviation Address 3103 9th Ave Dr NW Foreign Certificated Mechanic C. Certificate No. **Hickory** State NC Certificated Repair Station Country USA Zip Certificated Maintenance Organization FFGR870D I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge. Signature/Date of Authorized Individual____ Extended range fuel per 14 CFR Part 43 App. B 7. Approval for Return to Service Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is Approved Rejected FAA Flt. Standards Persons Approved by Canadian Maintenance Organization Manufacturer Department of Transport Inspector BY Other (Specify) Inspection Authorization **FAA Designee** Repair Station Certificate or Signature/Date of Authorized Individual Designation No. 05/09/2011 CR\$# FFGR 870D

NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. Description of Work Accomplished	·
(If more space is required, attach additional sheets.	Identify with aircraft nationality and registration mark and date work completed.)

USA N617CC

05-06-11

Nationality and Registration Mark

Date

Removed the following equipment:

1 ea Garmin GTX 330 Transponder p/n 011-00455-00 with mounting rack.

Installed the following equipment:

1 ea Garmin GTX 327 Transponder p/n 011-00490-00 with mounting rack.

B YIT TO

The GTX 327 Transponder was installed in the center instrument panel position vacated by the GTX 330 Transponder. The existing GTX 330 Transponder wiring and antenna were reused. The existing 5 amp transponder circuit breaker was removed and a new 3 amp transponder circuit breaker was installed. The GTX 327 Transponder was interfaced to the GNS 530 Com/Nav/GPS, the AR-850 blind encoder, and existing transponder antenna.

This is a follow on installation to STC Number SA00870WI issued to Garmin International for installation of a GTX 327 Transponder in a Piper PA-32 aircraft with the following deviation; 1. Aircraft make and model for this installation is a Cessna 421C.

All work was performed IAW Garmin GTX 327 Transponder Installation Manual No. 190-00877-02, Rev. N, April 2008; Cessna 421C Maintenance Manual No. D2515-23-13, Rev. 23, January 6 2003; AC 43.13-1B sections 5 through 17; and AC 43.13-2A sections 21,22,24 and 27.

An EMI test was conducted and the installed equipment does not interfere with any other installed system.

An Airplane Flight Manual Supplement was installed in the aircraft Pilots Operating Handbook.

Instructions for Continued Airworthiness and wire diagram are attached to this FAA form 337.

The aircraft equipment list was amended and the new aircraft weight & balance was computed and entered in the aircraft records.

RIVERHAWK AVIATION INC. 3103 9TH AVENUE DRIVE NW HICKORY, NC 28601 CRS FFGR870D

FLIGHT MANUAL SUPPLEMENT

FOR

CESSNA MODEL 421C

WITH

GARMIN GTX 327 TRANSPONDER

REGISTRATION NO. N617CC

SERIAL NO. 421C0897

This supplement shall be attached to the Cessna model 421C Flight Manual when the Garmin GTX 327 Transponder is installed in accordance with FAA Form 337, dated 05-06-11

The information contained herin supplements the information of the basic Flight Manual. For procedures, and performance data not contained in this supplement, consult the basic FAA approved Airplane Flight Manual.

APPROVED

05/06/2011

LOG OF REVISIONS

Rev. No. <u>Description</u> <u>Pages Revised</u> <u>Approved by</u> <u>Date</u>

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IV Normal Procedures	1
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Section I - General

The Garmin GTX 327 panel mounted Transponder is a radio transmitter and receiver that operates on radar frequencies, receiving ground radar or TCAS interrogations at 1030 MHz. The GTX 337 is equipped with IDENT capability that activates the Special Position Identification (SPI) pulse for 18 seconds

In addition to displaying the code, reply symbol and mode of operation, the GTX 327 screen will display pressure altitude, density altitude, and timer functions, depending on equipment connections and configuration selection.

Section II – Limitations

There is no change to the airplane limitations when this avionic equipment is installed.

Section III - Emergency Procedures

There is no change to the airplane emergency procedures when this avionic equipment is installed.

Section IV – Normal Procedures

Normal transponder operating procedures are described in the Garmin GTX 327 Pilot's Guide, p/n 190-00187-00, Rev. A, or later.

Section V – Performance

There is no change to the airplane performance when this avionic equipment is installed.

INSTRUCTIONS FOR CONTINUED AIRWORTHINESS

A/C Make: Cessna Model: 421C S/N: 421C0897 Reg. #: N617CC

Revision: Original Date: 05-06-11

System: Garmin GTX 327 Transponder

Item

1. This installation provides Transponder Mode A and C output capabilities when interrogated by ATC radar.

- 2. This installation consists of a GTX 327 Transponder mounted in the center instrument panel interfaced with the GNS 530 Com/Nav/GPS, the aircrafts blind encoder and transponder antenna.
- 3. Operation information can be found in the Garmin GTX 327 Airplane Flight Manual Supplement and the Garmin GTX 327 Pilots Guide p/n 190-00187-00 Rev. A or later.
- 4. There is no special servicing required for this installation.
- 5. Other than regulatory periodic functional checks the GTX 327 is an on condition component.
- 6. Troubleshooting information for the GTX 327 can be found in the Garmin GTX 327 Transponder Installation Manual Number 190-00187-02 latest revision.
- 7. Reference the Garmin GTX 327 Transponder Installation Manual Number 190-00187-02 latest revision for removal and replacement information.
- 8. There are no special servicing diagrams for this installation.
- 9. There are no special inspection requirements for this installation.
- 10. There are no protective treatments required for this installation.
- 11. Installation information can be found in the Garmin GTX 327 Transponder Installation manual number 190-00187-02 latest revision.
- 12. There are no special tools required for this installation.
- 13. This is not a commuter category aircraft.

INSTRUCTIONS FOR CONTINUED AIRWORTHINESS

A/C Make: Cessna Model: 421C S/N: 421C0897 Reg. #: N617CC

Revision: Original Date: 05-06-11

System: Garmin GTX 327 Transponder

<u>Item</u>

14. There are no recommended overhaul periods for this installation.

15. There are no Airworthiness limitations for this installation.

16. If a revision to this ICA becomes necessary a letter will be submitted to the local FSDO with a copy of the revised FAA form 337 and revised ICA.

MAJOR REPAIR AND ALTERATION

Form Approved OMB No. 2120-0020 11/30/2007

Electronic Tracking Number

U.S Departn Transportation Federal Avia Administrat	on ation	(4	\irfr	ame, Pov	erplar/	it, P	rope	ller, or <i>A</i>	pplian	ce)		For FAA Use Only
and dis		form. Thi	s repo	ort is required	by law (49	U.S.0						ereof) for instructions not to exceed \$1,000
Nationality and Registration Mark N617CC							_	Serial No. 421C0897				
1. Aircraft Make CESSNA								Model 421C			Series 421C	
Name (As shown on registration certificate) FAIRMONT AVIATION INC.							Address (As shown on registration certificate) Address 1660 PALMOUR DRIVE, SUITE AA-5 City GAINESVILLE State GA Zip 30501-6809 Country USA					VE, SUITE AA-5
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City <u>MAN</u> Zip <u>2910</u>		Country_	USA	State_SC		H	Certificated Repair Station Certificated Maintenance Organization			ganization	2000	46100
have	tify that the rep e been made it ished herein is	n accordan	ce wit	h the requiren	nents of P	art 43	of the	tem 5 above U.S. Federa	and descr Aviation F	ribed on the rever Regulations and t	rse or a	ttachments hereto information
Extended r per 14 CFF App. B				Signature/I JAMES				vidual LY 08, 20	08	A	<u> </u>	
						7. Ap	proval	for Return	o Service			
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DV ··	FAA Fit Sta	indards		Manufacture	r		Mainte	enance Orga	nization			erson Approved by Canadian epartment of Transport
BY	FAA Desigi	nee		Repair Statio	n	Х	Inspe	ction Authori	zation	1	ther (S)	pecify)
Certificate Designation 2660461	n No.		1	ature/Date of /IES W. ST				, 2008		AC		

NOTICE

8. Description of Work Accomplished			
(If more space is required, attach additional sheets. Id	dentify with aircraft nationality an	nd registration mark and	date work completed.)
•	N617CC		JULY 08, 2008
	Nationality and Registration	n Mark	Date
REPAIRED FUSELAGE - LOWER FWD BY REPLACING SKIN - LH 5213001-15, STRINGER - LH 5113109-54, ST 5113109-38, STRINGER - CENTER LOWER FWD 52130 BULKHEAD ASSY 5213025-21, STIFFENER 5213044-14	TRINGER - RH 5113109-55, ANG 001-8, DOUBLER - LH 5213045 4, CHANNEL 5113109-56.	GLE - LH 5213040-5, S ⁻ -3, BULKHEAD 521302	TRINGER ASSY - RH 1-3, BULKHEAD 5213026-7,
ALL WORK PERFORMED IN ACCORDANCE WITH CESPAGE 1-3/4 AND AC 43.13-18/2A SECTION 4, PARAGE	SSNA SERVICE MANUAL, STR RAPHS 4-50, 4-52 THRU 4-58.	UCTURAL REPAIR SE	CTION, CHAPTER 15, 15-30
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MAJOR REPAIR AND ALTERATION

OMB No. 2120-0020	Electronic Tracking Number
11/30/2007	For FAA Use Only
	FOI FAA USS OIIIY

(Airframe, Powerplant, Propeller, or Appliance) Administration INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a)) Nationality and Registration Mark Serial No. 42100897 NG17CC 1. Aircraft Series Name (As shown on registration certificate) Address (As shown on registration certificate) STE 2400 6065 ROSNELL RO NE FAIRMONT AVIATION INC 2. Owner State CA ATZANTA 30328-4011 3. For FAA Use Only 4. Type 5. Unit Identification Repair Alteration Unit Make Model Serial No. (As described in Item 1 above) AIRFRAME \mathbf{Z} POWERPLANT **Teledyne Continental Motors** GTSIO-520-L 292401-R PROPELLER Type APPLIANCE Manufacturer 6. Conformity Statement A. Agency's Name and Address B. Kind of Agency RAM Aircraft, Limited Partnership U. S. Certificated Mechanic Manufacturer 7505 Karl May Drive Foreign Certificated Mechanic C. Certificate No. City Waco State Texas Certificated Repair Station Airframe Class III, Powerplant Class I **United States** Zip 76708 Certificated Maintenance Organization VA1R551K I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge. Extended range fuel Signature/Date of Authorized Individual per 14 CFR Part 43 Арр. В Daniel L. Bailey 3/24/08 7. Approval for Return to Service Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is ✓ Approved Rejected FAA Fit. Standards Persons Approved by Canadian Manufacturer Maintenance Organization Inspector Department of Transport BY Other (Specify) **FAA Designee** Repair Station Inspection Authorization Certificate or Signature/Date of Authorized Individual Designation No. tunda Briles VA1R551K

Daniel L. Bailey 3/24/08

NOTICE

8. Description of Work Accomplished (If more space is required, attach additional sheets, Identify with aircraft national sheets).	onality and registration mark and date w	vork completed.)
	H617CC	3/24/08
	Nationality and Registration Mark	Date
	•	
Engine modified per Dwg. 1514, Rev. R dated 6/29/05 I/A/W STC SE6	,	
Relocated Turbo Oil Supply Line I/A/W RAM Dwg. No. 1224, Rev. H, o studs I/A/W Dwg. 1517, Rev. G dated 11/01/07 per STC SE8338SW.	dated 11/18/03 and installed locknu	ts on cylinder attachment
Installation mechanic must complete Block 1 and 2 on reverse side an Aircraft Registration Branch AFS-750, P.O. BOX 25504, Oklahoma Cit		ation Administration,
Negligible weight and balance change.		
Customer furnished with FAA approved Overhaul and Parts Manual S all alterations.	upplement(s) with instructions for c	ontinued airworthiness for
Pertinent details of the above installations are on file under project no.	4031/43588.	
End		
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Additional Shee	ts Are Attached	

United States Of America

Bepartment of Transportation - Nederal Abiation Administration

Supplemental Type Certificate

Number SE8338SW

This Certificate issued to

RAM Aircraft, Limited Partnership 7505 Karl May Drive Waco, TX 76708

certifies that the change in the type design for the following product with the limitations and conditions therefor as specified hereon meets the aircorthiness requirements of Part 13 of the CAR Regulations.

Original Product Type Certificate Namber

rake: Teledyne

Model GTS10-520

Description of Type Design Change

Install locknuts on cylinder attachment studs and thru-bolts, and install six additional cylinder attachment studs onto engine crankcases as detailed by RAM Drawing No. 1514 dated 08/19/91, or later FAA approved revision.

Linvitations and Conditions:

Compatibility of this modification with previously installed equipment must be determined by installer. If the holder agrees to permit another person to use this certificate to alter the product, the holder shall give the other person written evidence of that permission.

This certificate and the supporting duta which is the basis for approval shall remain in effect until surrendered, suspended, revoked or a termination date is otherwise established by the Administrator of the Federal Sociation Administration.

Dalo of application: October 01, 1991

Dalo reissued: October 8, 2001

Dalo of issuance May 19, 1992

Dalo amended:

By direction of the Sodministrator

(Signature)
S. Frances Cox, Manager
Special Certification Office,

Southwest Region

(Title)



US Department
of Transportation
Federal Aviation
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MAJOR REPAIR AND ALTERATION

Form Approved OMB No. 2120-0020 11/30/2007	Electronic Tracking Number
	or FAA Use Only

(Airframe, Powerplant, Propeller, or Appliance) INSTRUCTIONS: Print or type all entries. See Title 14 CFR §43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. §44701). Failure to report can result in a civil penalty for each such violation. (49 U.S.C. §46301(a)) Nationality and Registration Mark Serial No. N617CC 42100897 1. Aircraft Series CESSNA Name (As shown on registration certificate) Address (As shown on registration certificate)
Address 6065 ROSWEU RD NE FAIRMONT AVIATION INC 2. Owner ATLANTA 30328-4011 3. For FAA Use Only 4. Type 5. Unit Identification Repair Alteration Unit Make Model Serial No. AIRFRAME (As described in Item 1 above) \mathbf{Z} POWERPLANT Teledyne Continental Motors GTSIO-520-L 272484-R PROPELLER Туре APPLIANCE П Manufacturer 6. Conformity Statement A. Agency's Name and Address B. Kind of Agency RAM Aircraft, Limited Partnership U. S. Certificated Mechanic Manufacturer Address 7505 Karl May Drive Foreign Certificated Mechanic C. Certificate No. City <u>Waço</u> State Texas Certificated Repair Station Airframe Class III, Powerplant Class I **United States** 76708 Zip Country Certificated Maintenance Organization VA1R551K I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge. Extended range fuel Signature/Date of Authorized Individual per 14 CFR Part 43 Арр. В Daniel L. Bailey 3/24/08 7. Approval for Return to Service Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is ✓ Approved Rejected FAA Flt. Standards Persons Approved by Canadian Department of Transport Manufacturer Maintenance Organization Inspector BY Other (Specify) FAA Designee Repair Station Inspection Authorization Certificate or Signature/Date of Authorized Individual Designation No. VA1R551K Daniel L. Bailey 3/24/08

FAA Form 337 (10-06)

NOTICE

8. Description of Work Accomplished (If more space is required, attach additional sheets. Identify with aircraft nation	nality and registration mark and date wo	ork completed.)
.	N617CC	3/24/08
	Nationality and Registration Mark	Date
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		•
Engine modified per Dwg. 1514, Rev. R dated 6/29/05 I/A/W STC SE83	338SW.	•
Relocated Turbo Oil Supply Line I/A/W RAM Dwg. No. 1224, Rev. H, da studs I/A/W Dwg. 1517, Rev. G dated 11/01/07 per STC SE8338SW.	ated 11/18/03 and installed locknuts	s on cylinder attachment
Installation mechanic must complete Block 1 and 2 on reverse side and Aircraft Registration Branch AFS-750, P.O. BOX 25504, Oklahoma City	I mail one copy to the Federal Avia	tion Administration,
Negligible weight and balance change.		
Customer furnished with FAA approved Overhaul and Parts Manual Sulall alterations.	pplement(s) with instructions for co	ntinued airworthiness for
Pertinent details of the above installations are on file under project no. 4	4031/44911.	
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Additional Sheets	Are Attached	

United States Of America

Bepartment of Transportation - Nederal Abiation Administration

Supplemental Type Certificate

Number SE8338SW

This Certificate issued to

RAM Aircraft, Limited Partnership 7505 Karl May Drive Waco, TX 76708

certifies that the change in the type design for the following product with the limitations and conditions therefor as specified hereon meets the airworthiness requirements of Part 13 of the CAR Regulations.

Original Product Type Certificate Namber.

Teledyne



Description of Typo Design Change:

Install locknuts on cylinder attachment studs and thru-bolts, and install six additional cylinder attachment study onto engine crankcases as detailed by RAM Drawing No. 1514 dated 08/19/92, or later FAA approved revision.

Linulations and Conditions.

Compatibility of this modification with previously installed equipment must be determined by installer. If the holder agrees to permit another person to use this certificate to alter the product, the holder shall give the other person written evidence of that permission.

This certificate and the supporting duty which is the basis for approval shall remain in effect until surrendered, suspended, revoked or a termination date is otherwise established by the Administrator of the Foderal Sociation Administration.

Dalo of application: October 01, 1991

Dale reissued: October 8, 2001

Date of issuance May 19, 1992

Dalo amended:



By direction of the Administrator

Frances Cox, Manager

Special Certification Office,

Southwest Region

(Title)

6
U.S. Department of Transportation

MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved OMB No.2120-0020	Electronic Tracking Numbe
11/30/2007	

For FAA Use Only

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	ircraft	Nationa	ality and Re	gistration	Mark				Serial No				
	luiu.		7CC						4210	C-0897			
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Rep	air Al	lteration	Un	ıit	Make	e				Model			Serial No.
		X	AIRFRAI	\ME			_		(As described in item 1 above)			ve)	
			POWERPLANT										
			PROPEL	LLER			· 						
			APPLIAN	NCE	Туре						<u> </u>		
					Manufacturer								
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Name Address		N P BURK VERANDA				_ X		J.S. Certificated					Manufacturer
City		VERY BRAN		•	State GA	<u> </u>	+	oreign Certifica		nic			Certificate No.
Zip	30542		Country	USA	State GA	.	Certificated Repair Station Certificated Maintenance Organ			roanization		AP2	2887269IA
info	formation	n furnishe	ed herein	n is true a	tion made to the unit(nce with the requiremand correct to the bes	est of	dent s of f my	itified in item f Part 43 of t y knowledge	n 5 above	and descri	bed on ation Re	the rev ∋gulatio	/erse or attachments ons and that the
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BY	FA	AA Fit. Stan spector	idards		Manufacturer		T	Maintenance			Pers Dep	son Appo partment	oved by Canadian of Transport
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8. Description of Work Accomplished (If more space is required, attach addition	onal sheets. Identify with aircraft nationality an	d registration mark and date work completed.)
	N617CC	5-22-2008
	Nationality and Registration Mark	Date
Installed STC # SA10185SC Ha	rtzell Propellers	
Rev A, dated 20 Jan 2003. FAA a	-2UF/FC9587DB-7 Propellers in acca pproved RAM Airplane Flight Manual irplane Flight Manual. All work done i	Supplement AFMS 1097, Rev A
Weight and Balance Updated this	date.	
*******	******************* NOTHING FOLLOWS ********	******
		•
	•	
	Additional Sheets Are Attached	

Bepartment of Transportation - Nederal Abiation Administration

Supplemental Type Certificate

Number SA10185SC

This Certificate issued to

RAM Aircraft, Limited Partnership 7505 Karl May Drive Waco, TX 76708

Waco, 1A /0/08	
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certifies that the change in the type design for the follow	ving aroduct with the limitations and conditions
therefor as specified hereon meets the airconthiness repen	irements of Part 3 of the Civil Air Propulation
	Civil Kil Doegacacions.
Original Product Type Certificate Number A7CI	
Make, Cost	19
)"))
Model: 4210	
$((\)($	
Description of Type Design Change:	
Installation of Hartzell model HC-C3YN-2UF/FC9587D-7 or	+HC-C3YN-2UF/FC9587DB-7 propellers in accordance
with RAM Drawing No. 2584, Revision A, dated January 20.	2008, or later FAA approved revision. FAA approved
RAM Airplane Flight Manual Supplement AFMS 1097, Initia	al Release, dated August 7, 2003, or later FAA approved
revision is required.	
))
Limitations and Conditions:	
Compatibility of this design change with previously approved	modifications must be determined by the installer. If the
moder agrees to permit another person to use this certificate to	e alter the product, the holder shall give the other person
written evidence of that permission.	
-	
This certificate and the supporting data which is the bas	
surroudered surrounded mounted in the way	so for upproval shall remain in effect until
surrendered, suspended, revoked or a termination date is Federal Aviation Administration.	openious established by the Abdministrator of the
Swares Societies Sommistration.	L
Late of application . May 07, 2002	Dale reissued: August 27, 2003
Jule of issuance : August 07, 2003	64
2005 (y www.mer : August 07, 2003	Date amended:
aal a Ak	By direction of the Administrator
Ser All All All All All All All All All Al	I 1 Qual
	Tria Sillar
*	(Signature)
	S. Frances Cox
OMINISTRATO	Manager, Special Certification Office
	Southwest Region

(Title)

U.S. Department

MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved OMB No.2120-0020 11/30/2007	Electronic Tracking Number
For F	AA Use Only

Federal Aviation Administration INSTRUCTIONS: Print or type all entries. See Title CFR 43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 44701). Failure to report can result in a civil penalty for each such violation (49 U.S.C. 46301(a)). Nationality and Registration Mark Serial No. 1. Aircraft N617CC 421C-0897 Make Model Series CESSNA 421C Name (As shown on registration certificate) Address (As shown on registration certificate) 2. Owner Address 6065 ROSWELL RD NE STE 2400 FAIRMONT AVIATION INC City ATLANTA State GA 30328-4011 Zip Country 3. For FAA Use Only 4. Type 5. Unit Identification Repair Alteration Unit Make Model Serial No. X (As described in item 1 above) **AIRFRAME POWERPLANT PROPELLER** Type **APPLIANCE** Manufacturer 6. Conformity Statement A. Agency's Name and Address B. Kind of Agency Name AARON P BURKHART U.S. Certificated Mechanic Manufacturer Address 5013 VERANDA CT C. Certificate No. Foreign Certificated Mechanic City FLOWERY BRANCH State GA Certificated Repair Station AP2887269IA Zip 30542 Country USA Certificated Maintenance Organization I certify that the repair and/or alteration made to the unit(s) identified in item 5 above and described on the reverse or attachments hereto have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true, and correct to the best of my knowledge. Extended range fuel per Signature/Date of Authorized Individual 14 CFR Part 43 App. B 5-22-2008 7. Approval for Return To Service Pursuant to the authority given persons specified below, the unit identified in item 5 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is APPROVED REJECTED FAA Flt. Standards Person Appoved by Canadian Manufacturer Maintenance Organization Inspector BY Department of Transport

Other

Inspection Authorization

(Specify)

5-22-2008

Certificate or Designation No.

FAA Designee

Repair Station

Signature/Date of Authorized Individual

A		-	C	
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8. Description of Work Accomplished (If more space is required, attach add	d ditional sheets. Identify with aircraft nati	onality and registration m	ark and date work completed.)
	N617CC		5-22-2008
	Nationality and Registration Mark		Date
Installed STC # SA4592SW In	nproved Engine Cooling Baffle a	and Improved Exhau	st System Slip Joints
Installed improved engine coolin 1035 dated 1/1984 and 1153 Re 2A.	ng baffle and improved exhaust sev. A dated 1.23.1985. All work	system slip joints acc done in accordance	ording to RAM Drawings with AC43.13-1B and
****	******************** NOTHING FOLLOWS	5 *******	****
	·		
	Additional Sheets Are Attac	ched	

Bepartment of Transportation - Nederal Abiation Administration

Supplemental Type Certificate

Number SA4592SW

This Certificate issued to

RAM Aircraft, Limited Partnership 7505 Karl May Drive Waco, TX 76708

certifies that the change in the type design for the following product with the limitations and conditions therefor as specified hereon meets the airworthiness requirements of Part 3 of the Civil Air Poequlations.

Original Product Type Certificate Number :

Make Cessna

Model: 421c

Description of Type Design Change:

Installation of improved engine cooling baffle and improved exhaust system slip joints according to RAM Drawings 1035 dated 1/84, and 1153 Rev. A dated 1/23/85, or later FAA approved revision

Linvitations and Conditions:

Compatibility of this modification with previously installed equipment must be determined by installer. If the holder agrees to permit another person to use this certificate to alter the product, the holder shall give the other person wrriten evidence of that permission.

This certificate and the supporting data which is the basis for approval shall remain in effect until surrendered, suspended, revoked or a termination date is otherwise established by the Administrator of the Federal Aviation Administration.

Date of application January 05, 1982

Dale reissued: October 08, 2001

Date of issuance. January 27, 1982

Date amended: 7/24/85, 10/27/88 Rev. 2



By direction of the Administrator

S. Frances Cox, Manager

Special Certification Office,

Southwest Region

(Title)

6
U.S. Department of Transportation

MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved OMB No.2120-0020 11/30/2007	Electronic Tracking Number
For F	AA Use Only

Federal Aviation
Administration

INSTRUCTIONS: Print or type all entries. See Title CFR 43.9, Part 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 44701). Failure to report can result in a civil penalty for each such violation (49 U.S.C. 46301(a))

ii isti uctioni	is and disposi	sition of this for 9 U.S.C. 46301	orm. This report is required (a)).	ired t	oy law (49 U.	S.C. 44701). Failt	9-1 (or sub are to repor	sequer t can re	ent revision thereof) for esult in a civil penalty for
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2. Owne			gistration certificate)			Address (As show			
	FAIR	KMONIA	VIATION INC			Address 6065 R) NE STE	
		_				Zip 30328-		ountry	_ State GA
			3. F	For F	FAA Use Only	ly			
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Repair	Alteration	Unit	Mak	(e		Model	1		Serial No.
	x	AIRFRAME				(As described in	item 1 abov	ve)	
		POWERPLAN	IT .				•		
		PROPELLER							
		APPLIANCE	Type Manufacturer						
			6. C	onfo	ormity Staten	ment			
	y's Name and				B. Kind of				
	ARON P BURKI			_ x	U.S. Certificat	ted Mechanic			Manufacturer
	013 VERANDA (_	Foreign Certif	ficated Mechanic		C. C	Certificate No.
	LOWERY BRAN		State GA	_	Certificated Re	Repair Station		ΔĐ	2887269IA
Zip <u>30</u>	0542	_ CountryUS/	Α	-		Maintenance Organization	on	/\F4	2001209IA
informa	ation furnishe	ed herein is tru	eration made to the unity dance with the requirem ue and correct to the be	nents est of	dentified in ite s of Part 43 of f my knowledg	em 5 above and des	coribad an	the rev	rerse or attachments ons and that the
Extended rang 14 CFR Part 4		Signatu	re/Date of Authorized In	<u> </u>	low			<u>5</u> -	-22-2008
			7. Appro		for Return To				
Pursu the A	turrii istrator t	or the rederal	persons specified below A Aviation Administration	w, the	unit identified is	ed in item 5 was in	spected in	the ma	anner prescribed by
ву	FAA Fit. Stand Inspector	ndards	Manufacturer			ce Organization	Pers Dep	son Appo partment o	oved by Canadian of Transport
	FAA Design		Repair Station	X		Authorization	Other (Sp	pecify)	
	r Designation No	3 .	Signature/Date of Auth	iorize	d Individual				
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NOTICE

	N617CC			5-22-2008	
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talled STC # SA3721SW Vac talled vacuum pump cooling shi			221 "Vacuum Pumi	Cooling Shroud Installs	ion" Re
Dated 11 Feb 1987 and RAM D	rawing No. 1199, "Vacu	um Pump Cooling S	hroud Detail", Rev. E	3 dated 17 Feb 1987.	
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Bepartment of Transportation - Nederal Abiation Administration

Supplemental Type Certificate

Number SA3721SW

This Certificate issued to

RAM Aircraft, Limited Partnership 7505 Karl May Drive Waco, TX 76708

Waco, TX 76708
certifies that the change in the type design for the following product with the limitations and conditions therefor as specified hereon meets the aircoorthiness requirements of Part 3 of the Civil Air Regulations Original Product Type Certificate Number: A7CE
Make Cessna Model: 401/402/114/421 Series
Description of Type Design Change. Install vacuum pump cooling shroud in accordance with RAM Drawing No. 1221, "Vacuum Pump Cooling Shroud Installation", Revision B, dated February 11, 1987, and RAM Drawing No. 1199, "Vacuum Pump Cooling Shroud Detail", Revision B, date February 17, 1987, or later FAA approved revisions.
Eimitations and Conditions: Compatibility of this design change with previously approved modifications must be determined by the installer. If the holder agrees to permit another person to use this certificate to alter the product, the holder shall give the other person written evidence of that permission.
This certificate and the supporting data which is the basis for approval shall remain in effect until surrendered, suspended, revoked or a termination date is otherwise established by the Administrator of the Federal Sociation Sodministration.

Date of application. July 31, 1986

Date reissued: October 08, 2001

Date of issuance: February 23, 1987

Date amended:



By direction of the Administrator

S. Frances Cox, Manager Special Certification Office,

Southwest Region

(Title)

6
U.S. Department of Transportation

MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

For FAA Use Only

of Transportati	on	(.	Airfra	ame, Powerplant,	, Pr	op	eller, or A	(ppliance)		Fo	or FAA Use Only
Administration	n										
	TIONS: Prints and dispose violation (49	HUOH OF H	1115 101	III I IIIS IEDOM IS FEMI	43.9 uire	9, F d b	Part 43 Appe y law (49 U.	ndix B, and AC 43 S.C. 44701). Fail	3.9-1 (or sub ure to repor	sequer t can re	nt revision thereof) for esult in a civil penalty for
1. Aircra	Nationa	lity and Re						Serial No. 421C-0897			
	Make CESS	NA						Model 421C	·		Series
Name (As shown on registration certificate) FAIRMONT AVIATION INC								Address (As sho Address 6065 City ATLA Zip 30328	ROSWELL RE		
				3.	Fo	r F	AA Use Oni	у			
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4. 1	Гуре			5.	Uni	it Ic	dentification	1			
Repair	Alteration	Ur	nit	Ma	ake			Mode	el		Serial No.
x	AIRFRAME				(As described in item			item 1 abo	/e)		
		POWER	RPLANT	r							
		PROPE	LLER						- 1- 1- 1- 1- 1- 1- 1- 1- 1- 1- 1- 1- 1-		() (A).
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	ARON P BURK				_	x	B. Kind of	ted Mechanic			Manufacture
Address <u>50</u>	13 VERANDA	СТ			_	Ë		ficated Mechanic		C. C	Manufacturer ertificate No.
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nereto	nave been r	nade in a	accord	ration made to the un lance with the require le and correct to the b	mei	nte	of Part 43 o	fthe IIS Federal	escribed on Aviation Re	the revegulation	erse or attachments ins and that the
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				7. Appr	rova	al f	or Return T	o Service			
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8. Description of Work Accomplished						
(If more space is required, attach addition	nal sheets. Identify with aircraft nationality ar	nd registration mark and date work completed.)				
	N617CC	5-22-2008				
	Nationality and Registration Mark	Date				
Chafe damage was found in the LH Inboard	Engine Beam Outer Web at FS 115.00.					
Repaired chafe damage per Cessna Aircraft Company Repair definition S-421C-0897/01RD, Cessna 421C Maintenance Manual, Supplemental Inspection Documents SID 54-10-04 and SID 53-10-04, and AC43.13 1B.						
Blended chaffed area to a depth not greater	than .018 to a RMS 63 micro-inch finish.					
Eddy current inspection performed by Certification	ied Inspection Services FAA Repair Station >	(18R997N, no cracks noted.				
Fabricated repair doubler from .063 2024-T3	3 Clad Aluminum per Figure 3.2.					
Installed repair doubler using MS20470AD5	and NAS1097AD5 rivets and MIL-S-8802 CI	ass B2 sealant.				
Reinstalled Lower nacelle skin removed to g	pain access to patched area using standard p	practices of AC43.13-1B.				
See attached Cessna Aircraft Repair Definit	ion Dated 25 April 2008.					
******	************ NOTHING FOLLOWS *******	******				
·						
	Additional Sheets Are Attached					

U.S. DEPARTMENT OF TRANSPORTATION FEDERAL AVIATION ADMINISTRATION FAA Project No.								
STATEMENT OF COMPLIANCE WITH AIRWORTHINESS STANDARDS N/A								
AIRCRAFT OR AIRCRAFT COMPONENT IDENTIFICATION								
MAKE	MODEL NO.	TYPE (Aircraft, Engine, Propeller, etc.)	NAME OF APPLICANT/AUTH Cessna Aircraft Co	mpany				
Cessna	421C	Airplane	Wichita, Kansas DOA-230594					
LIST OF DATA								
IDENTIFICATION	IDENTIFICATION TITLE							
NOTES: 1. This Data approval is in support of Organizational Designation Project No. None and DOES NOT constitute DER approval of the data listed herein and is not valid for any other purpose or application. 2. In accordance with the Delegation Option Authority for Cessna Aircraft Company (DOA-230594-CE), when authorized below by the stamp of approval of the Executive Engineer, the listed data is FAA approved in support of a Major Repair 3. Approval is for design only, not installation. 4. Reference Repair Definition File Folder 421C-0897/01 in Structures Group Files. 5. Compatibility of this data with the aircraft configuration must be determined by the installer. Repair Definition S-421C-0897/01RD N/C REPAIR OF CHAFE DAMAGE IN THE LH INBOARD ENGINE BEAM — OUTER WEB								
PURPOSE OF DATA To approve listed data for repair of chafe damage to the LH inboard engine bear outer web (P/N 5054020-35) on Aircraft 421C-0897, registration number N617CC.								
APPLICABLE:REQUIREMENTS (List specific sections)								
	R. J. Lambe	_,	M.J. Nienh	aus				
CAR 3.171	3.174	3.295	3:307	3:307				
3/172	3.292	3.296						
3.173	3.294	:3:301	Dated May 15, 1956, as amended by 3-1 through 3-5 and 3-8					
CERTIFICATION: As directed by the Administrator and in accordance with the conditions and limitations of authorization under 14 CFR, data listed above and on attached sheets numbered have been examined in accordance with established procedures and found to comply with applicable requirements of the Airworthiness Standards listed. Recommend approval of these data X(We) Therefore: Approve these data								
SIGNATURE SY OF AUTHOR	IZED REPRESENTATIVE(S)	NAME	CLASSIFICATION	DATE				
0494		R. J. Lamberger	Structures	4/30/08				
Markel	Vuhan	M. J. Nienhaus	Structures	4/30/08				
			S MEAN THE POST	The second secon				



CESSNA AIRCRAFT COMPANY P. O. BOX 7704 WICHITA, KANSAS 67277-7704

STRUCTURES

MODEL NO: 421C REPORT NO: S-421C-0897/01RD

REPAIR DEFINITION

REPAIR OF CHAFE DAMAGE IN THE LH INBOARD ENGINE BEAM - OUTER WEB

REPORT DATE: April 25, 2008

PREPARED BY:

D. C. Abel

CHECKED BY:

J. Nienhaus

APPROVED BY:

R. J. Lamberger

Page i Report S-421C-0897/01RD Aircraft 421C-0897 (Unit 0897)

REVISIONS

LETTER	DATE	DESCRIPTION	BÝ	APPROVED
N/C	25April2008	Original release, ECR 099005.	DCA	See cover page.
				### 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1 1

1.0 AIRCRAFT DATA

Serial Number: 421C=0897

Unit Number: 0897

AW Date: 08/25/1980

6013.6 Hrs / N/A Ldgs

Reg. No.: N617CC

Date of Occurrence: On or about 04/11/2008

Owner/Operator: Fairmont Aviation

Repair Facility: abAviation LLC

SPECIAL NOTES

This Repair Definition covers only the damage defined in this document and it is the responsibility of the repair facility to assure that the defined damage agrees with the actual damage.

 All nondestructive inspections (NDI) specified in the Repair Definition must be performed by a qualified facility and meet the requirements defined in Appendix A and Supplemental Inspection Documents SID 54-10-04 and SID 53-10-04.

The repair described in the report satisfies applicable strength and fatigue, fail safe, and/or damage tolerance requirements. This repair also satisfies applicable lightning strike requirements. Structural substantiation and back-up data for this repair are contained in Engineering's Repair Definition File Folder 421C-0897/01.

The structural repairs designed herein for this aircraft do not affect the original structural inspection criteria (procedures or timing) as published in the Cessna Model 421C Service Manual.

The repair definition contained within this document has been found to comply with the following title CAR 3 regulations:

CAR 3.171, 3.172, 3.173, 3.174, 3.292, 3.294, 3.295, 3.296, 3.301, 3.307 as amended by Amendments 3-1 through 3-5 and 3-8 dated May 15, 1956.

2.0 DAMAGE DESCRIPTION

Chafe damage was found in the LH Inboard Engine Beam — Outboard Web (P/N 5054020-35) at FS 115:00. The final blended dimensions of the chafe damage in the LH Inboard Engine Beam — Outboard Web (P/N 5054020-35) covered an area of approximately 1.375 inch long, 1.25 inch wide, and 0.018 inch in depth (at the deepest point).

Figure 2.1 is a sketch showing the general location of the damage. Figure 2.2 are actual photographs showing the damage to the LH engine beam.

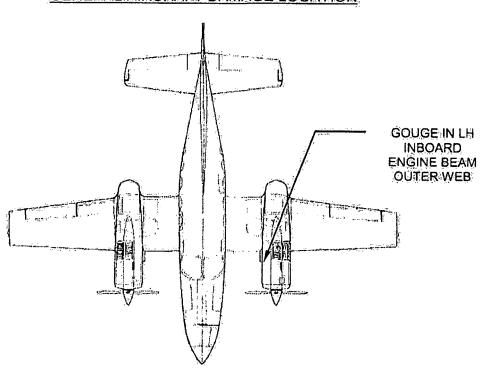
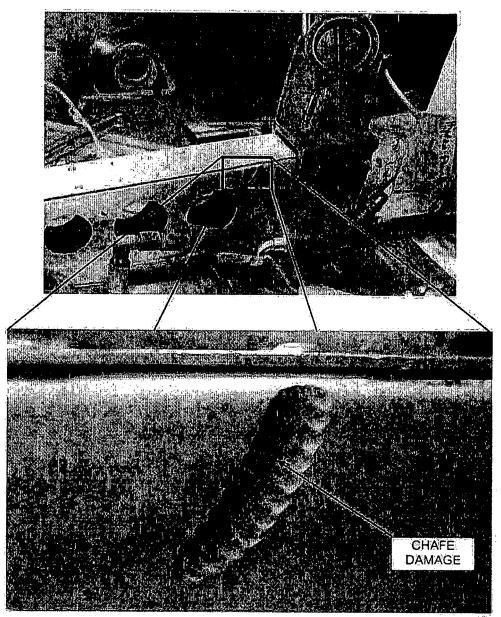


FIGURE 2.1:
GENERAL AIRCRAFT DAMAGE LOCATION

MODEL 421C AIRCRAFT

EIGURE 2.2: D'AMAGE PHOTO – LH INBOARD ENGINE BEAM



(VIEW LOOKING INBOARD/DOWN AT INNER LH ENGINE BEAM @ ES 115.00)



3.0 REPAIR DEFINITION

The chafe damage in the LH Inboard Engine Beam — Outboard Web (P/N 5054020-35) of the LH Inboard Beam Assembly (P/N 5054020-31) at FS 115.00 is to be repaired by inspecting for cracks and removing the chafe damage with a blend operation and a final polish and installing a repair doubler. This repair is to be performed in conjunction with Model 421C Supplemental Inspection Documents SID 54-10-04 and SID 53-10-04 instructions. Read all directions prior to beginning the repair and contact Cessna Customer Service if the repair cannot be installed as described below.

This repair requires that the final blended dimensions be sent back to Cessna Propeller Customer Service for further disposition.

Some of the following repair steps have already been performed on the original preliminary repair.

Repair per the following instructions:

3.1 REPAIR PREP

- First the LH engine will be removed from the aircraft. All weight will need to be taken off of the nacelle.
- Derivet the Lower LH Nacelle Skin Assembly (P/N 5154046-11) and the Lower Inboard LH Cap (P/N 5054020-29). This is to provide access to the inside of the LH Inboard Engine Beam Assembly (P/N 5054020-31). Remove rivets as shown in Figure 3.3 to allow installation of the -1 Repair Doubler. Note that the rivets on top of the engine beam will have to be removed also. Remove fasteners per Model 421C Service Manual.

3.2 INSPECT FOR DAMAGE

- Surface eddy current inspect for cracks in an approximate 1.0 inch region around the damaged location in the LH Inboard Engine Beam - Outboard Web (P/N 5054020-35) as shown in Figure 3.1.
 - Surface eddy current inspect per the attached eddy current document.
 (Reference Appendix A) and Supplemental Inspection Documents SID 54-10-04 and SID 53-10-04.

- Contact Cessna Propeller Customer Service if cracks or other anomalies are detected.
- o If no cracks or other anomalies are detected, continue with repair as described below.

3.3 REMOVAL OF DAMAGE

- Locally blend the chafe damage in the LH Inboard Engine Beam Outboard Web.
 (P/N 5054020-35) at FS 115.00 using 180 grit sandpaper to a minimum RMS 63 micro-inch finish as shown in Figure 3.1.
 - o Blend using a 0.25 1.0 in. radius (perpendicular to part surface).
 - o Do not blend any deeper than required to remove all surface damage.
 - Maximum blend depth shall not exceed .018 in. depth (.045) material remaining).
 - O Do not blend into the edge of the LH Upper Cap (P/N 5054020-7) as shown in Figure 3.1.
 - Report remaining material thickness and depth of material removed, and length and width of blended region to Cessna Propeller Customer Service for further disposition.
 - o Blending operation shall present a smooth and gradual transition between the reworked and non-reworked regions.

3.4 FINAL BLEND

- Locally polish the reworked area in the LH Inboard Engine Beam Outboard Web (P/N 5054020-35) at FS 115.00 using 320 grit sandpaper to a minimum RMS 63 micro-inch finish as shown in Figure 3.1.
 - o Polish using a 0.25 1.0 in. radius (perpendicular to part surface).
 - o Do not blend any deeper than required to achieve the above surface finish.
 - Maximum blend depth shall not exceed .018 in. depth (.045 material remaining).

- Do not blend into the edge of the LH Upper Cap (P/N 5054020-7) as shown in Figure 3.1.
- o Report remaining material thickness and depth of material removed, and length and width of blended region to Cessna Propeller Customer Service for further disposition.
- Blending operation shall present a smooth and gradual transition between the reworked and non-reworked regions.
- Final polish marks are to be in the Forward Aft direction.
- Clean, chem. film and epoxy prime per Model 421C Service Documents.

3.5 FABRICATION OF PARTS

- Procure an S3806-12 from Cessna Aircraft Company.
- Fabricate a -1 Repair Doubler from 0.063 in. thick 2024-T3 Clad aluminum sheet
 (S3806-12), per AMS-QQ-A-250/5, as shown in Figure(s) 3.2 thru 3.4.
 - Actual geometry of -1 Repair Doubler may vary to incorporate the size, type, and number of fasteners shown.
 - o The -1 Repair Doubler has a 0.19" Bend Radius.
 - Maintain 2D minimum edge distance to all included fasteners.
 - Break all sharp edges 0:03 to 0:06 in. radius.
 - o Clean, chem. film and epoxy prime the Filler per Model 421C Service Manual, "Protective Treatment Of Metal".

NOTE: If defects are detected then fabricate the -1 Repair Doubler from 2024-0 condition aluminum and heat treat to T3 condition per AMS-QQ-A-250/5 or procure a new 5054020-35 LH Outboard Web.

3.6 REPAIR INSTALLATION

- Install the -1 Repair Doubler as shown in Figure(s) 3.2 thru 3.4.
 - o Install size, number, and type of fasteners shown in Figure(s) 3.2 thru 3.4.

- Maintain 2D minimum edge distance at all fastener locations and 4D-8D spacing at all added fastener locations.
- o Install fasteners wet with Type I, Class B sealant per Model 421C Service Manual. Figure 3.4 illustrates the cross section of the parts stack up.
- Fay surface seal all mating surfaces per Model 421C Service Manual.
- Re-install any remaining fasteners that were removed from the Lower Skin to facilitate the installation of the Repair Doubler using B/P type and size fasteners. Install fasteners wet with Type I, Class B sealant per Model 421C Service Manual.

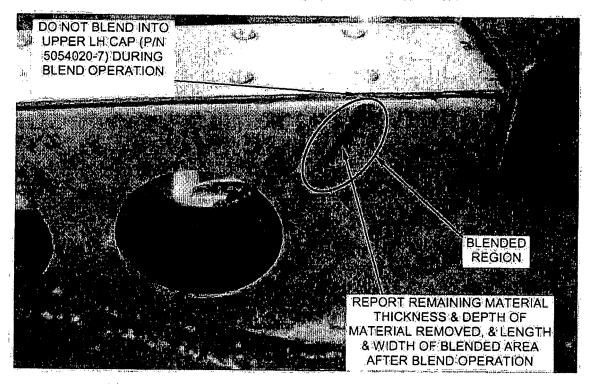
NOTE: It is permissible to substitute HL11VAZ Hi-Loks in place of NAS1097AD rivets. It is permissible to substitute HL10VAZ Hi-Loks in place of MS20470AD rivets. Use HL70 collars with above pins.

3.7 **GENERAL**

- Pre-assembly operations such as fitting, filing, drilling, dimpling and deburring shall be completed prior to cleaning.
- Fasteners are not to have an edge distance (measured from the center of hole to edge of part) of less than less than two times the hole diameter (2D) to the nearest part edge. The distance between fasteners is not to be less than four times the hole diameter (4D) and not more than eight times the hole diameter (8D). This note applies in all cases except where specifically noted in this report.
- Unless indicated otherwise below, all repairs and modification to the airplane are to conform to the Cessna Model 421C Service Manual.
- This repair is authorized for the stated Serial Numbered airplane only.
- This repair is for an unmodified airplane. Any non-Cessna modification that affects
 the aircraft gross weight, original design, or performance may invalidate published
 Continued Airworthiness requirements (i.e., inspection techniques and/or interval).
 Contact the STC Holder or originator of the modification for revised inspection
 criteria.

- If this Repair Definition Report (RD) is provided in support of a non-USA registered aircraft, acceptance of these findings is at the discretion of the civil aviation authority of the State of Registry. Compatibility of this data with the aircraft configuration must be determined by the installer.
- Prior to fastener removal, note the size, type, and location of any fasteners to be removed. This will facilitate later installation of similar fasteners (or oversized, as required, and as permitted by the Model 421C Service Manual or SRM).
- Seal all parts on installation, using the sealants specified in the Maintenance
 Manual, for each location. If no sealant or procedure is specified, Seal as instructed
 in the Maintenance Manual, using MIL-S-8802 Class B2 sealant on Assembly. Drive
 all fasteners wet with sealant.
- Break all edges to 0.03 to 0.06 inch radius, clean, chem. film and epoxy prime all bare aluminum surfaces and abrasive clean and epoxy prime all stainless steel surfaces per the applicable Model 421C Service Documentation.

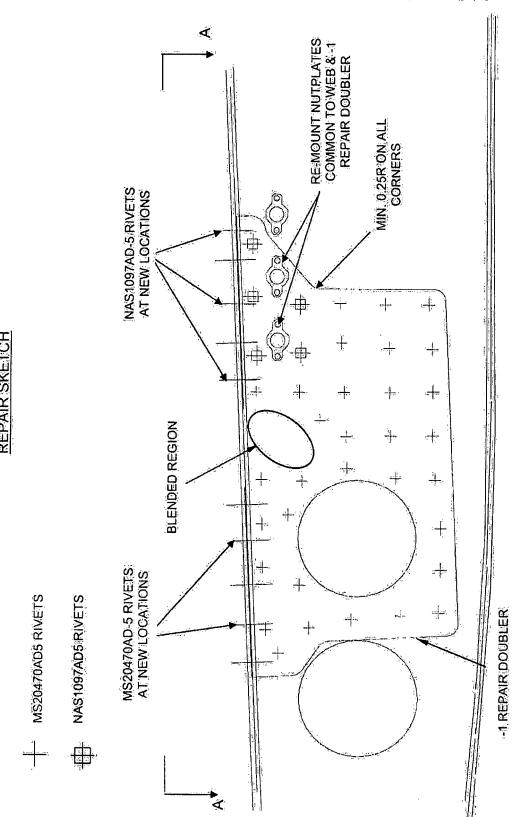
FIGURE 3.1 REPAIR DETAIL - LH ENGINE BEAM BLEND



(VIEW LOOKING INBOARD/DOWN AT INNER LH ENGINE BEAM)



FIGURE 3.2 REPAIR SKETCH



VIEW LOOKING INBOARD

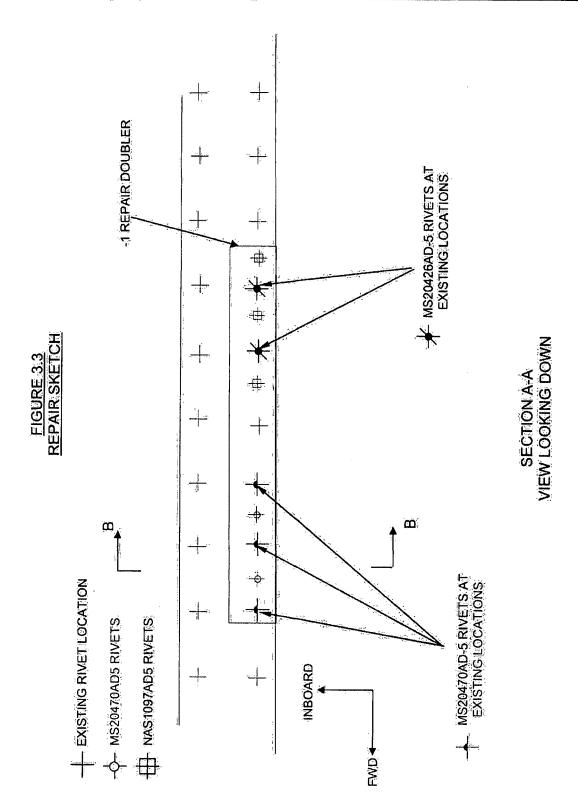
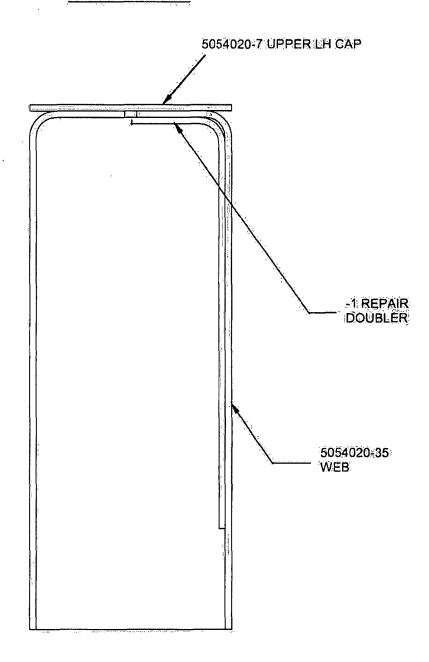


FIGURE 3.4 REPAIR SKETCH



SECTION B-B VIEW LOOKING AFT

APPENDIX A

EDDY CURRENT INSPECTION

Description

A. Eddy current inspection is effective for the detection of surface or near surface cracks in most metals. This method is accomplished by inducing eddy currents into the part and observing electrical variations of the induced field. The character of the observed field change is interpreted to determine the nature of the defect. Eddy current inspection can be applied to airframe parts or assemblies where the inspection area is accessible to contact by the eddy current probe. An important use of eddy current inspection is the detection of cracking caused by corrosion and stress around fastener holes with the fasteners installed.

2. Organizational Requirements

- A. The inspection must be preformed by an FAA approved Repair Station that holds a specialized rating for Eddy Current testing
- B. Personnel Requirements.
 - I. Personnel employed by a certified facility and engaged in nondestructive inspection on Cessna propeller equipped model airplanes shall be qualified and certified in the applicable method(s).
 - II. Personnel must be certified Level II or equivalent in the applicable method(s) in accordance with the employers qualification/certification program.

3. Instrument Requirements: Surface Inspection

A. Instrument Sensitivity:

1) Certain inspection techniques require the use of instruments that provide both phase and amplitude information on a storage cathode ray tube for impedance plane analysis. Impedance plane instruments may be substituted for metered instruments. Metered instruments shall not be substituted for impedance plane instruments where the ability to distinguish phase information is required.

- The instrument shall demonstrate a repeatable signal response that has a signal to noise ratio of greater than 3 to 1 for the test in which it is to be employed. Impedance plane instruments shall be able to resolve the signal within the guidelines shown in Figures 1 and 2.
- 3) The functional performance of the eddy current instrumentation shall be verified on an annual basis.

B. Probe Sensitivity:

- 1) The probe may have an absolute or differential coil arrangement.
- 2) The probe may be shielded or unshielded. A shielded probe is normally recommended.
- The probe shall have an operating frequency that produces the required test sensitivity and depth of penetration. For a surface inspection conducted on an aluminum part, the frequency should be 200 kHz (plus or minus 50 kHz).
- 4) Smaller coil diameters are more efficient in detecting cracks. A coil diameter of 1/8 inch is normally used.
- 5) For crack detection, the coil will usually contain a ferrite core.
- The probe shall not give interfering responses from handling pressures, scanning or normal operating pressure variations on the sensing coil which cause the signal to noise ratio to be less than 3 to 1.
- 7) Teflon tape may be used to decrease the wear on the eddy current probe coil. When Teflon tape is used, the instrument calibration must be verified.

4. Instrument Calibration Standards

- A) Nonferrous reference standards should be of an alloy having the same major base metal, basic temper and the approximate electrical conductivity of the material to be inspected.
- B) Reference standards shall have a minimum surface finish of 150 RHR or RMS 165.

- C) The dimensional accuracy of EDM notches shall be documented and traceable to the National Institute of Standards and Technology (NIST).
- In some cases a specially fabricated reference standard will be necessary to simulate a part's geometry, configuration, and/or the specific discontinuity location. Artificial discontinuities may be used in the reference standard.

Figure 1
Absolute Probe Calibration Range

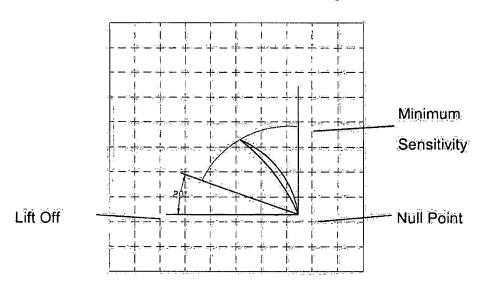
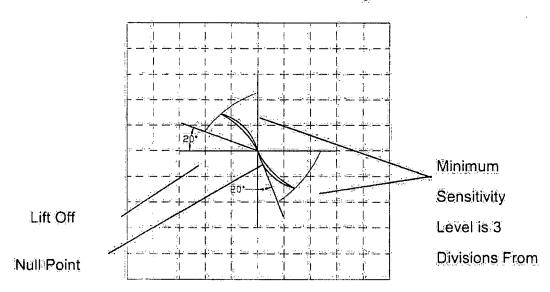


Figure 2

Differential Probe Calibration Range



3.8 EDDY CURRENT INSPECTION: SURFACE TECHNIQUES

1. Description

- A. This section presents the general requirements for the use of the eddy current method for detection of surface defects. This should be used in conjunction with specific instructions regarding the location for inspection and the types of discontinuities to be detected.
- B. Ensure compliance with the requirements described in the Eddy Current Inspection. General section.

2. Inspection Considerations

A. Temperature: Inspections shall not be performed until the temperature of the probe, the standard and the material have been allowed to equalize.

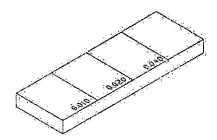
B. Surface Condition

- 1) Finish: The surface finish of the area to be tested shall be 150. RHR or RMS 165 or finer.
- Cleanliness: The areas to be tested shall be free of dirt, grease, oil, or other contaminants.
 - 3) Probe Contact: Eddy current inspection requires that good contact be made between the probe and the part unless a specific procedure requires a setoff. Mildly corroded parts must be cleaned lightly with emery cloth, heavily corroded or painted parts must be lightly abraded and cleaned locally in the area on which the probe will be placed.

3. Instrument Calibration

- A. The instrument shall be calibrated and operated in accordance with this technique and the manufacturer's instructions.
- B. Instrument calibration shall be performed prior to inspection. Calibration shall be checked at intervals necessary to maintain calibration during continuous use and at the conclusion of the inspection. The instrument shall be recalibrated if any part of the system is replaced or if any calibrated control settings are changed.
- C. A 0.020-inch surface notch shall be used for surface inspections unless otherwise specified. A typical eddy current surface reference standard with EDM notch depths of 0.010 inch, 0.020 inch, and 0.040 inch is shown in Figure 3.
- Adjust the instrument to achieve a minimum separation of three major screen divisions between the null/balance point and the appropriate reference notch. The signal from a differential probe should be considered peak to peak.
- E. Filters may be used to improve the signal to noise ratio.

Figure 3
Typical Surface Reference Standard



4. Inspection

- A. Whenever possible, the area of inspection shall be scanned in two different directions which are at scan paths 90 degrees to each other.
- B. Scan the inspection area at index increments that do not exceed the width of the eddy current test coil. The part edge shall be scanned as long as the response from edge effect does not mask the calibration notch response. Areas where edge effect is greater than the calibration notch signal shall not be inspected using eddy current.
- C. Whenever possible, fillets and radii should be scanned both transverse and parallel to the axis of the radius. The edge of the fillet or radius shall be scanned transverse to the axis of the radius.

5. Interpretation

- A. If an indication is detected, carefully repeat the inspection in the opposite direction of probe movement to verify the indication. If the indication persists, carefully monitor the amount of probe movement or rotation required to cause the instrument to move off maximum indication response.
- B. When the eddy current probe is centered over a crack, the crack response will be at maximum and any movement of the probe (+/- 0.020 inch, +/- 2 degrees) will cause the instrument to start to return to the original reading. Corrosion pits, foreign material, and out-of-round holes can cause and instrument response for 20 to 30 degrees of bolt hole probe rotation before the indication begins to return to the original reading.
- Unless otherwise specified, cracks shall be considered unacceptable.
- D. The end of a crack is determined using the 50 percent method. Scan the probe slowly across the end of the crack until a point is reached where the crack signal amplitude has been reduced by 50 percent. The center of the probe coil is considered to be the end of the crack.



3755 Industrial CT.,NW Suite16 Suwanee, GA 30024 FAA Repair Station XI8R997N

REPORT OF EDDY CURRENT EVALUATION

DD O I D O D						
PROJECT: Engine Support Beams					EVALUATION I	DATE: 04/14/08
CLIENT: ab Aviation LLC					PROJECT NUM	MBER: 15512
SPECIFICATION: Cessna A/C Company Mo	odel 421C SSI 54	1-10-04				P. O.: <u>1695</u>
FREQUENCY: 200		.6/1.0			ET INSTRUM	MENT: MIZ 21A
PHASE: 180	PERSIST:	N/A			CALIBRATION I	DATE: 6/06/08
GAIN: <u>42</u>	SWEEP:	N/A	_{D1}	EPORTED	CALIBRATION STANE	DARD: Zetec .008", .020", .040"
PROBE: Gen/Surface	FILTER:	N/A			Aaron Burkhart	
A/C Make Cessna	Model		421C	S	/N 421C-0897	Tail N617CC
Identification	ACC	CEPT	REJECT		Com	ments
# 1 Engine Beam Support Structure		V	N/A	No crac	ks noted @ time of	increation
	1			140 crac	ks noted a time of	mspection
# 2 Engine Beam Support Structure		N.	NI/A			
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# 2 Engine Beam Support Structure			N/A			

Form Page Number: 1 of 1

Form Revision No.: 3

Form Revision Date: 8-1-02



3755 Industrial CT NW Suite 16 • Suwanee, Georgia 30024 • (678) 730-2000 • (678) 482-9677 • www.mmelab.com

FIELD TEST REPORT

PROJECT: UTT Left Nacelle Inbd Beam

CLIENT: ab Aviation

EVALUATED BY: Kenneth Vaughn

CONTRACTOR: N/A

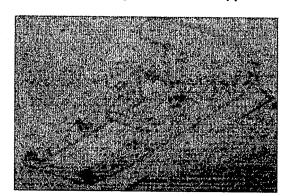
PROJECT NO.: 15527

DATE OF EVALUATION: 4-18-08

TYPE OF EVALUATION: Ultrasonic Thickness

SUMMARY OF WORK PERFORMED:

Perform a ultrasonic thickness inspection on the left nacelle -inboard beam in the area of chaffing, readings were taken in .025" increments fwd and aft of the blended area and, at the top and bottom of area. Thickness reading taken from the backside of the center area the average thickness is approx. .050".



FDW	Center	Aft	Тор	Bottom	
.052	.047	.050	0.	53	.053
.050	.045	.052			

Client:	Evaluator:	Kenneth Vaughn	

Form Page Number: 1 of 1

6
U.S. Department of Transportation
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MAJOR REPAIR AND ALTERATION

OMB No.2120-0020	Electronic Tracking Number
1/30/2007	

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NOTICE

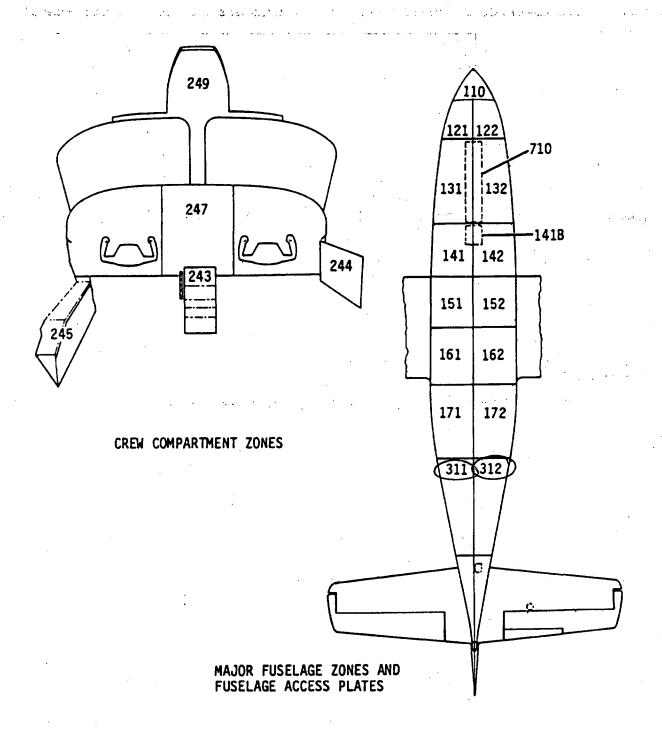
Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. Description of Work Accomplished (If more space is required, attach addition	nal sheets. Identify with aircraft nation	nality and registration ma	ark and date work completed)
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SECTION 00, PAGES 1-14			••
ADDITIONAL SHEETS ATTACHED			
REF. 1 = 1-60 PG. 4 FIG. 1 (SHEET 2) REF. 2 = 15-40 PG. 38 FIG. 33 REF. 3 = 15-40 PG. 16 FIG. 13			
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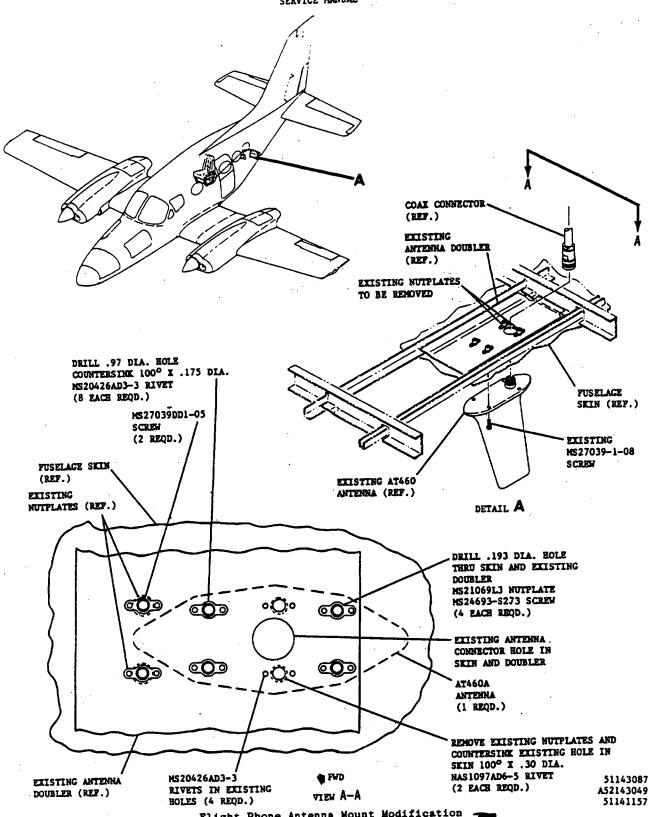
SERVICE MANUAL



Res. 1

Airplane Electrical Zoning Figure 1 (Sheet 2)

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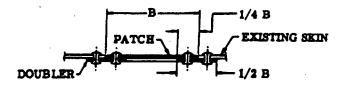


Flight Phone Antenna Mount Modification Figure 33

Ref. Z

15-40 Page 38 Oct/76

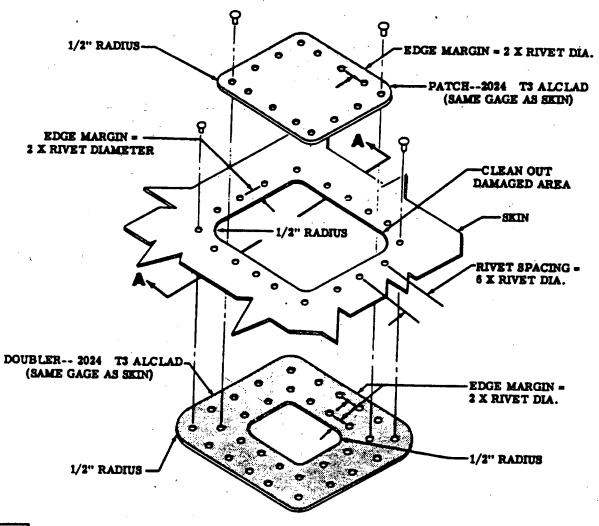
CESSNA ARCRAFT COMPANY 421 SERVICE NANUAL



SECTION THRU ASSEMBLED PATCH

A-A

RIVET	TABLE
SKIN GAGE	RIVET DIA.
. 020	3/32
. 025	1/8
.032	1/8
. 040	1/8
. 051	5/32



ORIGINAL PARTS
REPAIR PARTS
REPAIR PARTS IN CROSS SECTION

Ref. 3

Typical Rectangular Patch in Unpressurized Section Figure 13 (Sheet 1 of 2)

US Department of Transportation

Federal Aviation Administration

MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved OMB No. 2120-0020

For FAA Use Only

Office Identification

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July 11, 2005 Designation No. VA1R551K Travis D. Watts													

NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. Description of Work Accomp	onsnea
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(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Cessna 421C0897 N617CC Dated 07-11-05 Engine Previous Total: 1309.2

Relocated Turbo Oil Supply Line I/A/W RAM Dwg. No. 1224, Rev. H dated 11/18/03 and installed locknuts on cylinder attachment studs I/A/W Dwg. 1517, Rev. F dated 3/9/05 per STC SE8338SW.

Cust	omer furr	nished with	FAA approved (Overhaul	and Parts M	lanual Sเ	upplements	for all altera	ations.
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☐ Additional Sheets Are Attached

DIVERSIFIED EXECUTIVE ENTERPRISES, LLC 3655 ROSWELL ROAD, STE 202 ATLANTA, GA 30342

	FACS	SIMILE TRANSMITTA	L SHEET					
то: Ken Feist	FROM: Paul D'Agnese							
COMPANY:	DATE: . 3/14/05							
FAX NUMBER: 404-305-7226	TOTAL NO. OF PAGES INCLUDING COVING 1							
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DURGENT	X FOR REVIEW	□ PLEASE COMMENT	x please reply	☐ PLEASE RECYCLS				
NOTES/COMMENTS	- in the second							
Attn: Ken Feist								
From Paul D'Agi	nese							

Ken.

I am requesting a placement certificate as my original certificate has been lost for Cessna 421c-0897, registration #N617CC. The copy of log book showing last annual is being forwarded to you from Epps Aviation. Please feel free to call me on my cell at the number referenced below should you have any additional needs.

Paul D'Agnese Title Owner

404-307-1691

TOTAL TOTAL	TACH OR		
N617CC	Cessna 421C	421C-0897	ACTT: <u>5138.1</u>
(Registration Number)	(Aircraft Make and Model)	(Serial Number)	HOBBS: <u>5138.1</u>
Complied with the Ann Section's 2-41, 2-42, 2-	on's were complied with at this time: ual Inspection on this aircraft, Cessna 421C, So 43, and 2-44, ective's (AD's) were verified or complied w		th Cessna 421C Service Manual.
AD 96-20-67. C/W, by is 1370.4 at this time. T	performing heater system pressure decay test. he next test is due at 1470.4, or 24 months, who was a second	The test was satisfactory at this ti nichever occur's first.	me. Note: The heater hobbs reading
	due to Liberty brand fire extinguisher being ins by verifying this AD is not applicable to the pa		aircraft.
he following airframe mainten:	ance tasks' were accomplished at this time:		
	control system, in accordance with Cessna 4210 im tab cable tension as required, IAW Cessna 4		
Page 2, Paragraph (H).	•		
	rim tab travel check, IAW Cessna 421C SM. S	ection 5-30, Page 3, and Section	1-00, Page 2. Travel check was
	to be correct at this time. nserviceable Emergency Locator Transmitter (ELT) Battery, and installed a ser	viceable (new) Artex Et.T Battery
Pack, P/N 00-60-62, IA	W Artex Battery Replacement Instructions 57		
replacement date is Apr	il 2006. er wire for the tail navigation light, by splicing	the wire with an environmental of	ntice D/N Dd 26-26 and
repositioning the wire v	with a cable tie and stand off to prevent further of	chaffing, IAW AC43.13-1B, Para	ı. 11-167(b).
	, P/N 09-30450, on the LH alternate static air		
	the elevator trim tab control chain and cable, it aff strip, P/N MS21266-39, on a rib located in		
chaffing, IAW AC 43.1	3-1B. Para. 7-149(j).	•	•
	d the RH engine tachometer generator heat shid, Part D, and Cessna 421C SM, Section 7-80.		elding repair process. IAW AC
	nserviceable negative cable lug from the main:		rviocable (new) AWG #4 cable lug
	for the main aircraft battery, IAW AC 43.13-1		atta la contra de contra de la contra del la contra del la contra del la contra de la contra del la contra de la contra de la contra del la contra d
	nserviceable (broken) ground wire terminal on new) ring terminal, P/N MS25036-154, IAW :		ylinder valve cover area, and
Removed and replaced	the RH wing locker compartment light bulb (1)	ea.), P/N 1309, IAW Cessna 421	C SM, Section 13-32, Page 6.
Removed and reinstalle	d the EFI elevator assembly, to facilitate the ins on 5-30, Page 8, and AC 43.13-1B, Section 11	stallation of the inboard bonding	trap (1eà.), P/N FB-1/4", 1AW
	H aileron travel stop's in the LH and RH wing		21C SM, Section 5-10, Page 5.
Removed and replaced	the cabin light bulb's (5ea.). P/N 1495, located	on the LH side of the cabin area	as required. Performed an
Serviced the pilot's LH	c light bulb's. No defect's noted at this time, brake pedal master cylinder with MIL-II-5606	hydraulic fluid JAW Cosson 42	IC SM Section 2-60
Removed and replaced	the cockpit manifold pressure gauge post light!	bulb's (2ca.). P/N 6240-99-995-9	0118, IAW Cessna 421C SM,
Section 13-31, Page 1.4	Operational check was satisfactory at this time, w) hose clamp's (2ca.), P/N QS200M-44, on t	ha I U nic conditioner avacacetes	want blowm and door land.
under the floor board, b	elow the pilot's seat.	_	
Removed the existing u	nserviceable oxygen cylinder, P/N DOT-3HT,	S/N 53518, with regulator, P/N	72010-01, and installed a
regulator (Tea.), P/N 17) oxygen cylinder (1ea.), P/N 176018-115, S/N 2010-41, S/N C10363, IAW Cessna 421C SM	N 790764, MIg. Date 12-03, with 1 Section 6-41, Pages Land 5, N	a serviceable (overhauled)
regulator was received s	erviceable from AVOX, (W.O. 090226).		_
Printed a chaffed area o	n the inner surface skin of the RH engine cowli	ng 420DT, as required to prevent	corrosion.
removing the surface co	ind the lear drop panel, located aft of the vent or crosion, applying alodine to the bare surface's,	and painting epoxy primer over t	he entire surface area.
Adjusted the afteron cor	trol cable tension to 25 lbs., and performed a ti	ravel check as required. IAW Ces	sna 421C SM, Section 5-10, Pg. 4.
Removed and replaced t	he cabin entrance threshold floorboard panel () he nose landing gear uplock roller (1ca.), P/N	(ca.), P/N 5111226-31. NAS43HT5-108, and spaces / t) P/N NASARHTZ-111 1AW
Cessna 421C SM, Section	on 4-33.		
Removed the existing up C100098-6_TAW Cocco	serviceable RH fuel selector gear box, and instal 421C SM, Section 9-10, Page 6. Functional	alled a serviceable (new) fuel sel	ector gear box (Iea.), P/N
installed the upper cabin	door strut safety clip (Tea.), P/N A9002, TAW	Cessna 421C SM Section 3-11	Page 11.
Removed and replaced to 421C SM, Section 4-11.	he LH main landing gear uplock roller (lea.). I	P/N 5741157-1, and bolt (1ea.), !	P/N NAS6204-23D. IAW Cessna
	he RH main landing gear uplock roller (1ea.), t	P/N 5741157-1, and holt (1cm.). [PN NAS6204-23D TAW Cocens
421C SIVIL SOCION 4-11.			
the affected area's, as re-	in (rust) from inside of the forward cavity of the juired, IAW Cessna 421C SM. Section 1-80.	ELH and RH main landing gear t	railing link assemblies, and treated
Removed and replaced the	re L.H. main landing open trailing link accumbly	bearing's (2ea.), P/N M81934/2	-24A022, and washer's (2ea.)
11.4 33313-1, 174W C.C.S	na 421C SM, Section 4-11. Page 3		
4-41, tago 1.	ee LH and RH main wheel assembly keyed was		
Repaired a broke wire (1 shrink tubing for insulati	143B20), on the back of the cabin pressurization.	in switch, by soldering the wire ba	nck in place, and installing heat PAGE 1 OF 4
1 1	SUB-TOTALS this		

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Page No. TOTAL TACH OR Removed all of the existing unserviceable aileron, elevator, and rudder static discharger wick's and base's, and installed serviceable (new) static discharger wick's (8ca.), P/N DD1W, and base's (8ca.), P/N 5900012-1-1, IAW Cessna 421C SM, Section 14-90, Page 1, Step's A thru D. The aircraft IFR recertification was complied with at this time, by Northwest Arkansas Avionics, inc. (W.O. 0205). Note: For pertinent detail's of the IFR certification, see NWAA logbook entry, dated 4/22/04. Performed a compass swing on the aircraft standby compass, as required. Installed a PM Research radome boot (PM-56) onto the nose radome assembly after the paint repair process, IAW PM Research Installation Instructions Removed the existing unserviceable stall warning lift detector, P/N 186-9, S/N 1765, and installed a serviceable (overhauled) stall warning lift detector (1ea.), P/N 186-8, S/N 1112, IAW Cessna 421C SM, Section 13-40, Page 1. Note: The installed lift detector was received in a serviceable (overhauled) condition from Safe Flight Instrument Corporation (W.O. 337400). The following airframe structural repair tasks' were accomplished at this time: Repaired a small hole on the fuselage surface, forward of the RH nose landing gear door, at fuselage station 31.00, by removing the damaged area, and installing a 2024-T3 filler repair patch (1.0"x1.0"x.025), with a 2024-T3 internal doubler (3.0"x3.0"x3.0"x.032), IAW Cessna 421C SM, Section 15-40, Page 13, Figure 10. Repaired the working rivet's on the upper aft surface of the LH engine nacelle, by removing and installing the following serviceable (new) rivet's: P/N's MS20470AD-4, NAS1097AD-4, and CR3243-4, IAW Cessna 421C SM, Section 15-00, as required. Repaired a cracked cover panel (#14), located in the nose compartment, by removing panel, duplicating a new panel out of 2024-13 material (1.0"x1.5"x.020), and installing in nose compartment, IAW Cessna 421C SM, Section 15-00. Repaired minor surface corrosion around the vent on the lower fusclage surface, at fusclage station 289.94, by removing the corrosion, applying alodine to the bare surfaces, and painting the entire area with epoxy primer, IAW Cessna 421C SM, Section 15-50, Page 6, Para. Repaired minor surface corrosion around the tear drop panel on the lower fuselage surface all of the vent, at fuselage station 305.94, by removing the corrosion, applying alodine to the bare surfaces, and painting the entire area with epoxy primer, IAW Cessna 421C SM. Section 15-50, Page 6, Para. C. Repaired chaffing and a crack on the lower RH wing skin, located forward of the RH MLG forward trunnion bolt, by removing the damaged area, and installing a 2024-13 .032 doubler repair patch over the affected area, using EA9309 adhesive and rivet's, IAW Cessna 421C SM, Section 15-40, Page 39, Para, C. Repaired chaffing and a crack on the lower LH wing skin, located forward of the LH MLG forward trunnion bolt, by removing the damaged area, and installing a 2024-T3 .040 doubler repair with a 2024-T3 .032 filler, bonded together and to the aircraft using EA9309 adhesive and rivet's, IAW Cessna 421C SM, Section 15-40, Page 39, Para, C. Removed the working rivet's on the LH inboard flap skin, and installed serviceable (new) rivet's, on the LH inboard flap skin, IAW Cessna 421C SM, Section 15-00, Page 3. Repaired minor crosson on the wing leading edge skin, outboard of the stall warning vane, by burnishing the metal to contour, IAW Cessna 421C SM. Section 15-00. Repaired a crack on the floorboard skin, located forward of the pilot's rudder pedal's, where the alternate static air cable's route through, by removing the damaged skin, and installing a 2024-T3 doubler repair patch (7"x2"), over the affected area, using CR 3243 4-3 river's, IAW Cessna 421C SM, Section 15-40. Removed the werking river's on the surface area located forward of the RH main landing gear wheel well, and installed serviceable (new) rivet's (6ea.), P/N CR3212-4, IAW Cessna 421C SM, Section 15-00. Removed the working rivet's on the LH and RH wing to fuselage lower fairing's, cleaned and rescaled the area's, and installed serviceable (new) rivel's (as required). P/N's NAS1097AD-5. MS20426AD-4, and CR3242-4, IAW Cessna 421C SM, Section 15-00 and Section 1-Repaired a crack in the aft fuselage tailcone assembly, IAW Cossna 421C SM, Section 15-40, Page 1 Removed and replaced nutplate's (2ea.), P/N MS2107591.3, under engine nacelle panel #621AT, IAW Cessna 421C SM, Section 15-00. Repaired a challed forward stiffener, located at 410DT, on the LH engine cowling, by installing (2ea.) stainless steel doubler repair's. (1"x5"x.020), at the affected area, using Monel double flush rivet's and EA9309 epoxy adhesive, IAW Cessna 421C SM, Section 15-40. Repaired a chaffed all stiffener, located at 410DT, on the L11 engine cowling, by blending out the wear damage and corrosion, and installing (3ea.) .020 stainless steel dimpled internal doubler repair's, (1 1/8"x15"), (1 1/8"x12"), and (1 1/8"x15"), using Monel double flush rivet's (3-3), NAS1097AD4-3 rivet's, and EA9309 epoxy adhesive, IAW Cessna 421C SM, Section 15-40. Installed (4ca.) .020 stainless steel chaff plate's, (2"x12"), (2"x13"), (2"x6"), and (1 1/8"x14") over the affected area's of the LH engine cowling at location 410DT, using NAS1097-3-3 river's and EA9309 epoxy adhesive. IAW Cessna 421C SM. Section 15-40. Installed a rivet into the support angle to frame, under the floorboard, next to the ventilation duct, at fusciage station 200.75, IAW Cessna 421C SM, Section 15-00, Repaired the disbonded tip on the nose radome assembly, by removing the crosion boot, and applying a repair to the affected area. IAW Cessna 421C SM, Section 15-40, Page 3. Repaired a crack on the upper composite section of the RH main landing gear door assembly, by stop drilling the crack on both end's, cleaning out the length of the crack, tilling all honeycomb void's using cpoxy adhesive EA960F, and applying fiberglass overlay using the same epoxy adhesive, IAW Cessna 421C SM, Section 15-10, Page 10. Repaired the damaged skin around the lighter assembly in the eargo area, by removing the damaged skin area, and installing a 2024-T3 .032 aluminum doubler and filler repair patch over the affected area, IAW Cessna 421C SM, Section 15-40. Page 39. Repaired the double row of loose rivet's around the wing panel #632AB, outboard of the gear door, by removing the double row of rivet's. cleaning the scalant around the hole's, and installing serviceable (new) rivet's P/N NAS 1097AD-6-5, IAW Cessna 421C SM, Section 15-00. Scaled the repaired area, IAW Cessna 421C SM, Section 1-80. Repaired a row of loose rivet's on the LH nose door hinge assembly, by gaining access under the nose baggage compartment floor. removing the loose rivet's, and replacing with AD solid rivet's (14ca.), IAW Cessna 421C SM. Section 15-00. Repaired several loose river's on the upper surface of the RH wing, between the fuselage and engine nacelle, by removing the loose river's and installing serviceable (new) rivet's, P/N's CR3214-5 and CR3243-4, IAW Cessna 421C SM. Section 15-00. PAGE J OF 4 SUB-TOTALS this page

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	•	4. Qf	i ine nase	wheel wen	LH 3NG	KIII KCCIS	on's, mst	alline stamless	Strei plate (approx	the shim's (2ea.), P/N's 50	13049-1 and 5013049-	
i		4. on the uses wheel well LH and RH keclson's, installing stainless steel plate (approx. 3"x5"x.035) on the other side of the keeison, using AD-5 rivel's and epoxy adhesive EA9309, IAW Cessna 421C SM, Section 5-40. Required a shuffed LH wine lower for and										
	•	Repaired a chaffed LH wing lower forward spar cap, located above the LH main landing gear door assembly, by removing the existing unserviceable spar cap, duplicating a new spar cap from 2024-13 .063 aluminum, and installing on the aircraft, using Hi-Lok fastener's										
		and river's, IAW Acrodesign Aircraft Engineering Report No. 5054-1, IR for fastener substitution. Note: Adjusted the LH MLG door assembly as required to prevent further chaffing. Performed a landing gear operational check as required. The operational check was										
		12261	noiv as re	equired to p this time.	revent lu	ifther chaf	ting. Per	formed a landir	ng gear operational c	heek as required. The oper-	ational check was -	
<u> </u>	The	following a	irframe	alteration	was acc	omplished	d at this	time:				
	•	Installed 23ea. HL19PB6 fastener's and HL70 collar's, in the place of 23ea. NAS1055-6 fastener's in the LH lower forward spar, between wing station 159.00 and wing station 190.00, in accordance with Rocky Howard Engineering Report No. 5054-1, Revision IR, and										
-		COCUMCINEU WITH FACE FORM \$110-3 113160 April 70 7003										
	•	The aircraft weight change is negligible, and the aircraft weight and balance is not affected. For pertinent detail's of the above alteration, see Rocky Howard Engineering Report No. 5054-1, Revision IR, and Rose Aircraft Services.										
	Allai	Inc. FAA Form 337, Dated 04-21-2004. I airframe inspection's, maintenance tasks', structural repair tasks', and airframe alteration's were performed in accordance with Cessna 421C region. Region Laurent Factors (2016).										
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	THE	ancian	, 2117172	ıme, or a	ppnai	ace ider	itified	above was	renaired and i	spected in accorda	noo with	
	regu	mations (n the t	cuerai A	(VIZILIO	n Aom	inistra	tion and is	approved for r	eturn to service. Pe	rtinent details of	
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		ı	Remo	ved and	reins	talled	RH bra	ake back p	lates using n	ew hardware. Remo	oved	
			and	reinstal	led L	H brake	back	plates wi	th new lining	s, P/N 066-06600, olts, P/N 103-1180	1	
	_		and	washers	P/N 0:	95-1020	0. A)	ove work	performed IAW	C421C Service		
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U.S Department of

MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved -OMB No. 2120-0020

For FAA Use Only

Federal Aviation						FSW-F	Signification			
. and dispositi	ONS: Print or type all entries. See FAR 43.9 ion of this form. This report is required by law h violation (Section 901 Federal Aviation Act	n (49 U.S.C	ppendix B, and A 2: 1421). Failure	AC 43.9-1 (o to report ca	or subsequent revision result in a civil pe	ion thereof) for instr enalty not to exceed	encitour 1\$1,000			
•	Make CESSNA			Model 421C						
1. Aircraft	Serial No. 421C0897			Nationality and Registration Mark NS17CC						
2. Owner	Name (As shown on registration certificate COB CAPITAL CORP	e) •	Address (As shown on registration certificate) . 6065 ROSWELL RD NE # 2400 ATLANTA GA 30328-4011							
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		4	. Unit Identifica	ation	•	•	5. Type			
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have been	at the repair and/or alteration made to the unit or made in accordance with the requirements rue and correct to the best of my knowledge.	of Part 43 c	ed in item 4 abov of the U.S. Feder	e and descr al Aviation F	ribed on the reverse Regulations and tha	e or attachments he at the information fu	ereto rnished			
Date 04-21-2004			Signature of Au		ividual	B-	· · · ·			
•	and the second	7. Apr	orovai for Retur	n to Servic	e · · · · · · · · · · · · · · · · · · ·					
	e authority given persons specified below, the of the Federal Aviation Administration and is			s inspected EJECTED						
BY	A Fit Standards Manufacturer pector		Inspection Auth	4-7		her (Specify)	•			
FA	A Designee X Repair Station		Person Approve Canada Airwort	hiness Grou	ıp .					
Date of Approval 04-21-2004	or Rejection Certificate or Designation No.		Signature of Au R. J. BAKEF		ividual	132 ·				

NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. Des	Description of Work Accomplished	
	(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark an	d date work completed.)
CESSI	ESSNA, 421C S/N 421C0897 DATE: 04-21-2004.	
	INSTALLED 23ea HL19PB6 FASTENERS AND HL70 COLLARS IN PLACE OF 23ea NAS1055-6 FAST	ENERS IN THE LEFT LOWER
1. IN	FORWARD SPAR, BETWEEN W.S. 159 AND 190 IN ACCORDANCE WITH ROCKY HOWARD ENGI	NEERING REPORT NO. 5054-1
R	REV. IR AND DOCUMENTED WITH FAA FORM 8110-3 DATED APRIL 20, 2004.	
2. W	WEIGHT CHANGE IS NEGLIGIBLE, AIRCRAFT WEIGHT AND BALANCE IS NOT AFFECTED.	
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	(2)(b),-3.172, 3.173, 5.174, 196, 3.301, 3.306, 3.307	3.1151i, 3.1156, 3.1	23, 3,222, 3,223, 3	294.	
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with Umitations of a data listed above and	apointment under Pent 186 om ettechet sheds munike	of the Federal	Awision Regulation	ons, inedim	
accordance with estat of the Federal Awlation					
II (Ma) Therefore	X Approve these	pproval of these d	E-2005		
Signature(s)) of Design	ated Engineering Rehits	metikas ID	esigneian Riumber	(2)	
R.M. Howard, Jr.	The Marie		DSRT-710134-9	3 W/	Shuciues

US Department of Transportation

Federal Aviation Administration

MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved OMB No. 2120-0020

For FAA Use Only

Office Identification

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dispo	sition of thi	s form. This	report	entries. See FAF t is required by la Aviation Act of 1	w (49	FAR U.S.C	43 Appendix : 3. 1421). Failu	B, and AC are to repo	3.9 ort ca	9-1 (or subseque un result in a civi	ent revision there il penalty not to e	eof) for instr exceed \$1,0	ructions and 000 for each		
1. A	ircraft	Make						Model							
Cessna						•			421C						
		Serial No.		421C089	7			Nationality and Registration Mark N6174C							
2. Ov	wner	NAME (As	on registration o	ertifica	te)		Address (As shown on registration certificate)								
		Cobb Capital Corp.							6065 Roswell Rd NE # 2400 Atlanta, GA 30328 486 EIVED						
For FAA Use Only MAR 1 9 2002										2					
4. Unit Identification FAA Type											Туре				
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Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and ate work completed.)

<u>Cessna 421C0897 N6174C</u> Airframe Total Time: 4473.4 Dated 02-07-02

Engine crankcases modified per Dwg. 1514, Rev. H dated 2/2/99 I/A/W STC SE8338SW.

Relocated turbo oil supply lines I/A/W RAM Dwg. No. 1224, Rev. F, dated 2/2/99 and installed locknuts on cylinder attached studs I/A/W Dwg. 1517, Rev. C dated 6/8/98 per STC SE8338SW.

Installed Power Pac Spoilers kit SP400 per STC SA4913NM

Inspected previously installed A.T.S V.G's per installation manual IM92-6A. Inserted Flight manual Supplement ATS 93-02a in aircraft P.O.H. Installed placard P/N 1C0067000-02 on instrument panel per STC SA00015SE.

Installed Floscan Fuel Flow transducers per Dwg. 1084, Rev. N, dated 10/20/00 in accordance with STC SE5726SW.

Installed Shadin Fuel Flow Indicating System; per Dwg. 1084, Rev. N, dated 10/20/00 I/A/W STC SA5796SW.

Installed range-marked EGT, CHT, oil temperature and pressure gages I/A/W Dwg. 1152, Rev. B dated 8/27/97 per STC SA5878SW. Flight manual Supplement dated 2/1/85 furnished.

Installed Alcor EGT system per manufacturer's instructions I/A/W STC SA522SW.

Installed improved engine cooling baffles and improved exhaust system slip joints per Dwg's. 1009, Rev. J, dated 11/5/96 and 1001, Rev. U dated 04/06/01 I/A/W STC SA4592SW.

Installed spring loaded induction hose clamps per Dwg. 1171 Rev. B, dated 5/24/00 I/A/W STC SE3767SW.

Installed new NDM oil cooler P/N CAM 646685 I/A/W STC SE00577NY.

Installed vacuum pump cooling shroud on left and right engine vacuum pumps I/A/W RAM Dwg. 1221, Rev. K, dated 10/27/98 and RAM Dwg. 1199, Rev. H, dated 9/30/96 per STC SA3721SW.

New empty weight and balance computed.

Customer furnished with FAA approved Overhaul and Parts Manual Supplements for all alterations.

Customer furnished with FAA approved Flight Manual Supplements for all operations.

Pertinent details of the above installations are on file under work order no. 5450.

----End----

US Department of Transportation

Federal Aviation Administration

MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved OMB No. 2120-0020

For FAA Use Only

Office Leading FSDO

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000

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Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

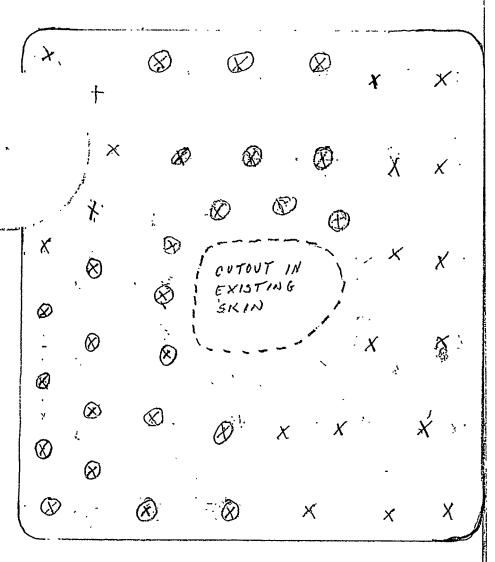
8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

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X = EXISTING RIVETS Ø = ADDITIONAL RIVETS

 US Department of Transportation Federal Aviation Administration

MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved OMB No. 2120-0020

11

For FAA Use Only 604

Office Identification IND FSDO

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

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2-14-96 N 6174C: Date

Certified previously installed Trimble TNL 2000Approach GPS system for IFR enroute flight.

This units operational revision status is NAV 2.573, GPS 4.02-1214.

The aircraft was test flown and found to meet the requirements for IFR enroute and terminal navigation per AC 20-138.

Test Pilot signature certifying IFR accuracy requirements.

Date: 2-14-96

KENNETH K. TALHELM #2406901

A revised TNL 2000Approach Pilots guide, P/N 81449 rev E dated April 14, 1995 or later version, has been placed in the aircraft.

The FAA Approved Flight Manual Supplement accompanying this Field Approval is issued to this aircraft.

Removed placard stating "GPS not to be used for navigation.

Weight and Balance change is negligible.

US Department of Transportation Federal Aviation Administration

MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved OMB No. 2120-0020

For FAA Use Only

Office Identification
GL-11 / IND FSDD

INSTRUCTIONS: Print or type all entries. See FAR 43.9, FAR 43 Appendix B, and AC 43.9-1 (or subsequent revision thereof) for instructions and disposition of this form. This report is required by law (49 U.S.C. 1421). Failure to report can result in a civil penalty not to exceed \$1,000 for each such violation (Section 901 Federal Aviation Act of 1958).

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Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued confermity with the applicable airworthiness requirements.

8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

N 6174C: Date 2-14-96

Removed Trimble TNL 2000T Satellite Navigation System.

The following equipment is installed in the instrument panel:

Installed this date Trimble TNL-2000 Approach GPS system TSO C129(A1) in the instrument panel, in accordance with STC SA09005SC. The CDI interface, and annunciator lights are installed as required for IFR non precision approach approval.

The altitude reporting system was interfaced with the GPS using a Transcal IF-RS232C-2 adaptor on the previous installation.

The altitude reporting system was fully checked in accordance with Appendix 1 of AC 43.6A per FAR 91.411 and 91.413.

The GPS antenna was installed in accordance with Advisory Circular 43.13-1A chapter 11, sections 2,3,5, and 7 and 43.13-2A, Chapter 3, paragraph 38 a and b; The GPS receiver was installed in accordance with Advisory Circular 20-138, paragraph 8c(2)(i), 8c(2)(ii). as a follow on to the above STC. A placard was installed stating "GPS is not used for navigation" pending FAA approval for IFR navigation.

Weight and balance and equipment list has been revised for this installation.

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	that the repair ar en made in acco ed herein is true i				Manufacturer oit(s) identified in its of Part 43 of the b	em 4 above .S. Federal	and described on Aviation Regulati	1		
Date	1-13-95	ind correct	to the best	or my	Signature of Auth	orized Indi				
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BY	A Designee	x -	Station	+	Person Approved b	v Transport	1			* * *
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Weight and balance or operating invitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

Description of Work Accomplished
(If more space is required, attach additional sheets, identify with aircraft nationality and registration mark and date work completed.)

Removed existing defective Aerosonic Fuel Flow system.

Installed this date Shadin Co. Inc. model 910532TD Fuel Flow Indicating system in adcordance with Airframe STC # SA607GL type certificate # A7CE and Engine STC # SE612GL type certificate # E7CE. This system was installed per Shadin company report/installation manual # 4034 and 4043.

All work was performed in accordance with Advisory Circular 43.13-2A, chapter 11, Section 2, installed in spaces provided by the aircraft manufacturer.

The Shadin Digiflo-L operating manual was placed in the Aircraft Flight Manual.

Weight and Balance change is negligible.

Tal Additional Shoots Are Albached.

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AIRFRAME POWERPLANT PROPELLER APPLIANCE	Type	As described	Model	ement	•			Altera

D. I certify that the repair and/or alteration made to the unit(s) identified in item 4 above and described on the reverse or attachments have been made in accordance with the requirements of Part 43 of the U.S. Federal Aviation Regulations and that the information furnished herein is true and correct to the best of my knowledge.

Manufacturer

Date

Signature of Authorized Individual

Certificated Repair Station

P.O. BOX 560235AY MONTVERDE, FL. 34

MIKE MCMURRER

7. Approval for Return To Service

264455**865AP**

Pursuant to the authority given persons specified below, the unit identified in item 4 was inspected in the manner prescribed by the Administrator of the Federal Aviation Administration and is X APPROVED

REJECTED

Other (Specify) FAA Fit. Standards Manufacturer_ Inspection Authorization . BY Person Approved by Transport Canada Airworthness Group FAA Designee Repair Station Date of Approval or Rejection Certificate or Designation No. Signature of Authorized Individual

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FAA Form 337 (12-88)

anges shall be entered in the appropriate aircraft record. An alteration must be Weight and balance or operating limitation stanges shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assore continued conformity with the applicable airworthiness requirements.

8. Description of Work Accomplished th aircraft nationality and registration mark and date work completed.) (If more space is required, attach additional sheets, Identify INSTALLED VORTEX GENERATOR SYSTEMS ON THE WING AND VERTICAL STEBILIZER, IN ACCORDANCE WITH STC SA00015SE AND THE INSTALLATION INSTRUCTIONS MANUAL IM92-6 AND FAA APPROVED COPY OF AERONAUTICAL TESTING SERVICE DRAWING LIST NO. ATS-421C. BOTH DATED MAY 2, 1994. THIS MODIFICATION INCLUDES REDUCE VMCa AND STALL SPEEDS AND A GROSS WEIGHT INCREASE. REVISED FLIGHT MANUAL WITH COPY OF FAA APPROVED AIRPLANE FLIGHT MANUAL SUPPLEMENT REPORT NO. WEIGHT CHANGE NEGLIGIBLE. ATS-93-02a, DATED AUGUST 26, 1993. --END--For FAA Use Only 1 nolle illiesbutint . 5. Type Script No. Repair we chooks they in the discover also were WHICH DAY STREET MONTHERE 100 n.¥ARSSGac 18 IL 32755 RECEIVED 022 012 013 43 GTC.0 5 1994 which is all the manner to reach the statement 04 145 INDFSDO godayhediy Ayedia**ka**aat 016 भर भार भार भार भारती है। बिर विशेष स्थापन कि 1 45 m Additional Sheets Are Attached +U.S.GPO:1889-0-883-171

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	Aviation Co.		ited Mechanic]		
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Muncie	, Indiana 47308		Repair Station		AJVR13	15C
have bee	hat the repair and/or alteration made in made in accordance with the requi d herein is true and correct to the bes	rements of Part 43 of the	tem 4 above and descr	ibed on the rever Regulations and	se or attachme that the infor	ents hereto mation
Date		Signature of Au	thorized Individual			
1	1-11-94	Kennet	h K. Talhel	in # 24069	901	
		7. Approval for Return	To Service			``
	the authority given persons specific or of the Federal Aviation Administra		lied in item 4 was ins VED 🔲 REJECTED		anner prescri	bed by the
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PA	A Designee X Repair Station	Person Approved Canada Airworth				
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FAA Form 33						

8. Description of Work Accomplished

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

San TED OF	iously installed Tri	mble TNL 2000T GPS system	
for IFR flight	• The second sec		1.00
This units ope	rational revision sta	atus is NAV 2.250, GPS 1.1	.2
The aircraft w requirements f AC 20-121a, pa	as test flown and for or IFR enroute and to ragraph 9.	und to meet the erminal navigation per	FS -1
Test Pilot sig	nature certifying IF	R accuracy requirements.	
KILTI	· ·	ate: 11-11-94	
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A revised TNL August 19, 199 aircraft.	2000T Pilots guide, F 3 or later version, h	P/N 80817 No rev dated has been placed in the	*## ###
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and dispos for each su	CONS. Print or type all entries. See FAR 4 lition of this form. This report is required to the violation (Section 901 Federal Aviation)	3.9, FAR 43 Appendix B, and AC 43.9-1 (or subseq by law (49 U.S.C. 1421). Failure to report can result on Act of 1958).	juent revision thereof) for instructions tin a civil penalty not to exceed \$1,000
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	grade to the second of the control o	ing a second control of the control	A. S.
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BOX 118	9, Muncie Airport	U.S. Certificated Mechanic Foreign Certificated Mechanic	
nuncie,	Indiana 47308	X Certificated Repair Station	AJVR135C
		Manufacturer	
D. I certify the have been furnished	at the repair and/or alteration made to the made in accordance with the requirement herein is true and correct to the best of	e unit(s) identified in item 4 above and described c ents of Part 43 of the U.S. Federal Aviation Regul my knowledge	on the reverse or attachments hereto attons and that the information
Date	The state of the s	Signature of Authorized Individual	7
11	-11-94	WILLIAM E ROUNDTREE	
Pursuant to t	7. the authority given persons specified b r of the Federal Aviation Administration	Approval for Return To Service elow, the unit identified in item 4 was inspected	d in the manner prescribed by the
FAA	Fit. Standards Manufacturer	Inspection Authorization Other (Special	<u></u>
	Designee Repair Station	Person Approved by Transport Canada Airworthiness Group	
ate of Approve	or Rejection Certificate or	Signature of Authorized Individual	/

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NOTICE

Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Removed this date, one Tomorrow Appolo 612 Loran C system. Removed this date one cessna electric clock. Installed 1 LC-8 digital electric clock. Installed this date Trimble TNL 2000T (TSO-C129) Satellite Navigation System. The GPS antenna is TSO-C129 status. Installed Annunciators and CDI interconnections IFR approval. The GPS CDI output was connected through a failsafe switching system to the pilot HSI with ILS priority and controlled by an annuciated switch. Installed Transcal Industries altitude encoder to RS232C interface adapter model # IA-232C-2, to the existing encoder and the trimble GPS receiver. The GPS antenna was installed in accordance with Advisory Circular 43.13-2A, Chapter 3, paragraph 38 a and b; The GPS

receiver was installed in accordance with Advisory Circular 20-121A Chapter 7 paragraph a 1 thru 5 in spaces provided by the manufacturer and A placard was installed stating "GPS NOT APPROVED FOR IFR. All work was performed using manufacturers schematics, drawings, and in accordance with Advisory Circular 43.13-2A, chapter 1, page 5 and chapter 2, paragraphs 21, 22, and 23 installed in spaces provided by the aircraft manufacturer. The altitude reporting system was fully checked in accordance with Appendix 1 of AC 43.6A per FAR 91.411 and 81.413.

All systems were checked per manufacturers installation and testing procedures.

Weight and Balance records have been revised.

END-

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☐ Additional Sheets Are Attached

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MAJOR REPAIR AND ALTERATION (Airframe, Powerplant, Propeller, or Appliance)

Form Approved OMB No. 2120-0020

For FAA Use Only
Office Identification

Administration						40 Ç	PSpo	<u>پ</u>
and disposit	ONS: Print or type all entries. See FAR ion of this form. This report is required h violation (Section 901 Federal Aviation	by law (49 l	J.S.C. 1421). Fa					
	Make Cessna	9.		Model	421C	-		
1. Aircraft	Serial No. : 421C0897			National	ity and Registratio	n Mark	N6 174C	
Sekstos a A.	Name (As shown on registration cert	ificate)		Address	(As shown on regi	stration c	ertificate)	
2. Owner	World Tower Company		a kirila Albertus 1964 use	May	Box 405 field, KY.	420	56	
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Smyrna Air Center Hangar 62! Smyrna Airport Smyrna, TN. 37167			U.S. Certificate Foreign Certific Certificated Rep Manufacturer	ated Mechanic RG4 air Station Rad			491M o ted Airframe	
D. I certify th	nat the repair and/or alteration made to poster image broance with the requirer higher is a rue and correct to the best of	ments of Pa of my know	identified in iter art 43 of the U. liedge.	S. Federal	Aviation Regulation	ne reverse	or attachme	nts hereto
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Administrato	the authority given persons specified to the Federal Aviation Administration	below, the n and is	e unit identified ▼□ APPROVE	initem 4 D⊟R	4 was inspected in EJECTED	the man	ner prescrib	ed by the
WARSES	Fit. Standards Manufacturer Manufacturer	1 1	ection Authoriza		Other (Specify)		,	
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Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. Description of Work Accomplished

(If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

INSTALLED NEW NARCO AR850 BLIND ENCODER IN SPARE INSTRUMENT HOLE IN PILOT'S INSTRUMENT PANEL. WIRED ENCODER TO EXISTING ARC 400 SERIES TRANSPONDER.

THE ABOVE INSTALLATION WAS DONE AS PER MANUFACTURERS INSTALLATION MANUAL AND A.C.43.13-1A, CHAPTER 11, SECTIONS 1 AND 2, CHAPTER 15, SECTIONS 1 AND 2; A.C.43.13-2A, CHAPTERS 1 THRU 3.
RECERTIFIED STATIC SYSTEM AND RAN ENCODER CORRELATION TESTS PER FAR 91.411. ALL WORK ON FILE AT SMYRNA AIR CENTER ON WORK ORDER

#14251.

WEIGHT MOMENT ARM OLD WEIGHT & BALANCE: 544. 152.03 827568.9 AR850 ENCODER

NEW EMPTY WEIGHT: 5444.19

NEW CENTER OF GRAVITY: 152.03

NEW MOMENT: 827658.1

THE NEW CENTER OF GRAVITY FALLS WITHIN THE LIMITS AS SPECIFIED IN THE FLIGHT MANUAL. THE NEW ELECTRICAL LOAD DOES NOT EXCEED 80% OF THE AIRCRAFT CAPABILITY. IT IS THE RESPONSIBILITY OF THE PILOT TO INSURE CORRECT WEIGHT AND BALANCE BEFORE EACH FLIGHT. -----BND--

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FSDO Tennessee

Additional Sheets Are Attached

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Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. DESCRIPTION OF WORK ACCOMPLISHED (If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Removed ARC R1048A Nav receiver, RTA876A DME transceiver, C876A DME control, 800 RNAV and RN868A computer.

Installed II Morrow 612 Loran C IPR receiver, A23 Loran C antenna, King KN 64 DME and ARC R1048B Nav receiver, FC-02 Flag adapter.

A switch, relays and a blue annunciator light are provided for Loran sharing of CDT with the #2 Nav system. The CDT is automatically switched to VOR/LOC kode when the 8 Nav radio is switched to ILS frequency.

All work performed in accordance with manufacturers installation instructions.

with manufacturers installation instructions.

A test flight was performed to certify compliance with AC 90-45A for VPR. IFR. Enroute operation by Land branch Lic. No. 1322 365 on 15 1 1986.

Aircraft has been placarded "Loran C not certified for Approaches."
An FAA approved Flight Manual supplement and Apollo II Model 612 IFR
operation manual have been included in the aircraft records.
Log book entry reference this form 337. Aircraft weight and balance
report and equipment list revised.

RECEIVED 144 07

ADDITIONAL SHEETS ARE ATTACHED

d.

	U.	S. DEPARTMENT OF	FRANSF	PORTATION		Form Ruda	Approved et Bureau No	. 04-R06	0.1	
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	(Airframe, Powerplant, Propeller, or Appliance) AGL-GADO-10									
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Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

8. DESCRIPTION OF WORK ACCOMPLISHED (If more space is required, attach additional sheets. Identify with aircraft nationality and registration mark and date work completed.)

Removed ARC E 1048A Nav Receiver, RTA876A DME transceiver, C876A DME control, 800 RNAV and RN878A computer.
Installed II Morrow 612 Loran C IFR receiver, A23 Loran C antenna King KN64 DME and ARC R1048B Nav receiver.

King kN64 IME and ARC R1048B Nav receiver.

A switch, relays and a blue annunciator light are provided for Loran sharing of CDI with the P Nav system. The CDI is automatically switched to VOR/LOC Mode when the P Nav radio is switched to the IIS frequency. All work performed In accordance with AC 43.13-1A-2A and in accordance with manufacturers installation istructions.

Aircraft has been placarded "Loran-C not certific for IFR flight."
Pending FAA approval of flight manual supplement and IFR certification.

Log book entry references this form 337. Aircraft weight and balance report and equipment list revised.

MG | APS | RECEIVED 6 1986 МЗ M4 GADO-10 07 M5 | CL1

ADDITIONAL SHEETS ARE ATTACHED

C . FAA AC 72-4906

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FAA Form 337 (7-67)

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Weight and balance or operating limitation changes shall be entered in the appropriate aircraft record. An alteration must be compatible with all previous alterations to assure continued conformity with the applicable airworthiness requirements.

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CHECK IF RECORDS IN COMPLIANCE WITH X FAR 91.173

DATE OF APPLICATION

8-25-80

AIRCRAFT LISTING (Gire page Not 1)

N/A C. AIRCRAFT OPERATION AND MAINTENANCE RECORDS

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LIMITED

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DEPARTMENT OF TRANSPORTATION FEDERAL AVIATION ADMINISTRATION APPLICATION FOR AIRWORTHINESS CERTIFICATE

APPLICATION IS HEREBY MADE FOR: (Check applicable items)

PROVISIONAL (Indicate class)

RESTRICTED (Indicate operation(s) to be conducted)

EXPERIMENTAL (Indicate operation(s) to be conducted)

SPECIAL FLIGHT PERMIT Industre operation to be conducted from complete Section VI or VII as applicable on reverse side)

Cessna Aircraft Company

A7CE Rev. 19

AIRCRAFT SPECIFICATION OR TYPE CERTIFICATION DATA SHEET (GITE No. and Revision No.)

A 7 C.E. ROY 1.0

Cessna

6: ENGINE BUILDER'S NAME (make)

9. PROPELLER BUILDER'S NAME (make)

Continental

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M. Gann, Quality Control Manager

81-25-82

Form Approved Budget Bureau No. 04-R0058 INSTRUCTIONS—Print or type: Do not write in shaded areas; these are for FAA use only. Submit original only to an authorized FAA Representative. If additional space is required, use an attachment. For special flight permits complete Sections II and VI or VII as applicable. AIRCRAFT MODEL DESIGNATION 4. YR MFG. FAA CODING 421C ENGINE MODEL DESIGNATION GTSIO-520-L O. PROPELLER MODEL DESIGNATION 3FF32C501/90UMB-0 TRANSPORT GLIDER BALLOON A 1 X STANDARD AIRWORTHINESS CERT. (Indicate category) X NORMAL UTILITY ACROBATIC SPECIAL AIRWORTHINESS CERTIFICATE (Check appropriate items) CLASS I CLASS II AERIAL SURVEYING 3 AGRICULTURE & PEST CONTROL 2 ADVERTISING WEATHER CONTROL 6 FOREST (Wild life conservation) -5 PATROLLING OTHER (Specify) EXHIBITION AMATEUR BUILT RESEARCH AND DEVELOPMENT CREW TRAINING 6 MKT: SURVEY 5 RACING TO SHOW COMPLIANCE WITH FAR FERRY FLIGHT FOR REPAIRS, ALTERATIONS, MAINTENANCE OR STORAGE EVACUATE FROM AREA OF IMPENDING DANGER OPERATION IN EXCESS OF MAX CERTIFICATED TAKE-OFF WEIGHT 3 PRODUCTION FLIGHT TESTING DELIVERING OR EXPORT MULTIPLE AIRWORTHINESS CERTIFICATE (Check appropriate Restricted Operation and Standard or Limited as applicable above) IF DEALER, CHECK HERE A. REGISTERED OWNER (As shown on Certificate of Aircraft Registration) West K-42 Highway, P.O. Box 7704 ALDRESS 67277 wichita, Kansas B. AIRCRAFT CERTIFICATION BASIS. (Check applicable blocks and complete items as indicated) AIRWORTHINESS DIRECTIVES (Clock of all applicable AD's complied with and give latest AD No.) Technology PO 15 12 Latest AD No. 1 Issue: 80-15-13

SUPPLEMENTAL TYPE CERTIFICATE (Lin number of each STC incorporate) SA3003WE EXPERIMENTAL ONLY - Enter Lours flown since tas acrefulte issued or renewal N/A D. CERTIFICATION—I hereby certify that I am the owner (or his agent) of the aircraft described above; that the aircraft is registered with the Federal Aviation Administration in accordance with Section 501 of the Federal Aviation Act of 1958, and applicable Federal Aviation Regulations; and that the aircraft has been inspected and is airworthy and eligible for the airworthiness certificate requested.

A. THE AIRCRAFT DESCRIBED ABOVE HAS BEEN INSPECTED AND FOUND AIRWORTHY BY: (Comp section only if FAR 21 163 (d) applies) CERTIFICATED MECHANIC (GILL Certificate No.) CERTIFICATED REPAIR STATION (Gue Certificat FAR PART 121 OR 127 CERTIFICATE HOLDER (Gire Certificate No) 3 ó 2 AIRCRAFT MANUFACTURER (Gir Name of Firm) 5 SIGNATURE TITLE DATE (Check ALL applicable blocks) I find that the aircraft described in Section I or VII meets the requirements for XX The certification requested, or 4 Amendment or modification of its current airworthiness certificate.
Section VII was conducted by: FAA Inspector, operations to historian diagrams FAR 65, Inspection for a special flight permit under , ____ FAR 121 or 127, or ____ FAR 145. FAA INSPECTOR'S SIGNATURE DISTRICT OFFICE DESIGNATION MANUALTER, CB-3 CE EMDO skowa 3-0-43 8-25-80 Raymond M. Rowder FAA Form 8120--6 (7-70)

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5	C. GIVE QUANTITY OF CERTIFICATES REQUIRED FOR OPERATING NEEDS	SIGNATURE								
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影	B. Current Operating Limitations Attached	required)								
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≧34	X D. Current Weight and Balance Information Available in Aircraft	FAR CAR (Original attached)								
II. AIRWORTHINESS DOCUMENTATION (FAA uso only)	E. Major Repair and Alteration, FAA 337 (Attach when required)	J. Current Airworthiness Certificate Issued in Accordance with								
۳-	F. This Inspection Recorded in Aircraft Records	X FAR 21.1830 per 21.273 (Copy uttuched)								
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